

**CITY OF RALEIGH**

**Existing Zoning Map**

**Z-9-11**

**SC (0.81 ac)  
&  
SC w/PDD  
& PBOD (10.93 ac)**

**to**

**SC CUD w/ PBOD  
(8.126 ac)**

**and**

**SC CUD  
(3.613 ac)**

**11.74 acres**

Public Hearing  
July 19, 2011  
(October 17, 2011)

475 Feet



	<ul style="list-style-type: none"> <li>• Upon development of Section 1 of the property, pedestrian connectivity from existing grade to the Crabtree Valley Avenue will be provided</li> <li>• Actual location of Crabtree Valley Mall Connector Road will be established at the time of site plan approval</li> <li>• Along Creedmoor Road frontage of the property, parking is limited to two rows between ROW and the building</li> <li>• Provides for transit easement</li> <li>• Provides for restrictive covenant that allocates allowable development to all lots within 30 days of rezoning approval</li> <li>• Provides for connectivity and design coordination of streetscape elements in non-PBOD portion (Section 3) to that of the PBOD area (Sections 1&amp;2)</li> </ul>
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### Issues and Impacts

<i>Outstanding Issues</i>	Staff comments on Streetscape Plan (Deferred by applicant to site plan review process. See attached memo).	<i>Suggested Conditions</i>	- None
<i>Impacts Identified</i>	<ul style="list-style-type: none"> <li>- Development of the subject property will add additional traffic to an area that, at present, is congested during peak periods.</li> <li>- It is recommended that the applicant provide pedestrian access to Crabtree Creek trail to the north.</li> </ul>	<i>Proposed Mitigation</i>	- Conditions address traffic impacts by limiting land uses to the traffic capacities supported by the existing infrastructure

### Public Meetings

<i>Neighborhood Meeting</i>	<i>Public Hearing</i>	<i>Committee</i>	<i>Planning Commission</i>
3/7/11	7/19/11	Date: NA	Date: 8/9/11 approved with revised conditions

**Valid Statutory Protest Petition**

#### Attachments

1. Staff report
2. Existing Zoning/Location Map
3. Future Land Use

## Planning Commission Recommendation

<i>Recommendation</i>	The Planning Commission finds that the proposed rezoning is consistent with the policies of the Comprehensive Plan, and recommends, based on the findings and reasons stated herein, that the request be approved in accordance with zoning conditions dated August 15, 2011 and Streetscape and Parking Plan (SSP-2-11) dated August 2, 2011.
<i>Findings &amp; Reasons</i>	<ol style="list-style-type: none"> <li>(1) That the proposed request is consistent with the regional mixed use category designated to the property by the future land use map. The site is recommended for a mix of uses and the proposed conditional use rezoning and the streetscape and parking plan seeks to permit a mix of uses and enhanced pedestrian amenities.</li> <li>(2) That the request is compatible with surrounding land uses and development patterns. The proposed zoning conditions address traffic and parking impacts, thus mitigating any potential adverse impacts to the surrounding uses.</li> <li>(3) That the request is reasonable and in the public interest. Rezoning would permit introduction of higher density residential uses and retail uses, which could be of service to the immediately adjoining institutional uses, and the larger surrounding area, thus furthering the goals of several Comprehensive Plan Policies.</li> </ol>
<i>Motion and Vote</i>	<p>Motion: Mattox            Seconded: Haq</p> <p>In Favor: Butler, Fleming, Harris Edmisten, Haq, Lyle, Mattox, Schuster, Sterling Lewis</p> <p>Opposed: Anderson</p>

This document is a true and accurate statement of the findings and recommendations of the Planning Commission. Approval of this document incorporates all of the findings of the attached Staff Report.

	08/9/11
Planning Director	Date
	Planning Commission Chairperson
	Date

Staff Coordinator: Dhanya Sandeep [dhanya.sandeep@raleighnc.gov](mailto:dhanya.sandeep@raleighnc.gov)



# Zoning Staff Report – Z-9-11

## Conditional Use District

### Request

<i>Location</i>	Eastside, southeast of its intersection with Crabtree Valley
<i>Request</i>	Rezone property from SC (0.81 acre) & SC w/PDD & PBOD (10.93 acres) to SC CUD w/PBOD (8.126 acres) and SC CUD (3.613 acres)
<i>Area of Request</i>	11.74 acres
<i>Property Owner</i>	Weingarten Investments Inc.
<i>PC Recommendation Deadline</i>	10/17/11

### Subject Property

	<i>Current</i>	<i>Proposed</i>
<i>Zoning</i>	SC (0.81 acre) & SC w/PDD & PBOD (10.93 acres)	SC CUD w/PBOD (8.126 acres) SC CUD (3.613 acres)
<i>Additional Overlay</i>	PDD & PBOD	PBOD (8.126 acres)
<i>Land Use</i>	Undeveloped	Commercial uses (max. indicated for each possible category)
<i>Residential Density</i>	624 total DU	575 total DU

### Surrounding Area

	<i>North</i>	<i>South</i>	<i>East</i>	<i>West</i>
<i>Zoning</i>	SC, SC CUD & O&I-2	Residential 6	Shopping Center	O&I-1, R-6
<i>Future Land Use</i>	Office & Residential Mixed use, Public Park and Open Space	Regional Mixed Use	Regional Mixed Use, Public Park and Open Space	Office & Residential Mixed use
<i>Current Land Use</i>	Offices	Multi-family residential	Crabtree Valley mall	Senior living community, residential uses

### Comprehensive Plan Guidance

<i>Future Land Use</i>	Regional Mixed Use
<i>Area Plan</i>	Crabtree Area Plan
<i>Applicable Policies</i>	Policy LU 2.6 Zoning and Infrastructure Impacts

	Policy LU 4.4 Reducing VMT through Mixed Use Policy LU 4.5 Connectivity Policy LU 5.1 Reinforcing the Urban Pattern Policy LU 5.2 Managing Commercial Development Impacts Policy LU 5.4 Density Transitions Policy LU 5.5 Transitional and Buffer Zone Districts Policy LU 5.6 Buffering Requirements Policy LU 6.4 Bus Stop Dedication Policy LU 7.1 Encouraging Nodal Development Policy LU 7.4 Scale and Design of New Commercial Uses Policy LU 7.6 Pedestrian Friendly Development Policy LU 8.10 Infill Development Policy LU 8.11 Development of Vacant Sites Policy LU 10.6 Retail Nodes Policy LU 7.5 High Impact Commercial Uses Policy ED 5.3 Creating Attractive Development Sites Policy UD 3.5 Visually Cohesive Streetscapes Policy UD 4.3 Improving Streetscape Design Policy UD 6.1 Encouraging Pedestrian-Oriented Uses Policy UD 7.3 Design Guidelines (1-26 policies) Policy AP-C2 Crabtree Area Hillside Policy AP-C4 Crabtree Mall Connections Policy AP-C5 Design Unity in the Crabtree Area
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### Contact Information

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<i>Applicant</i>	Lacy Reaves, 821-6704, <a href="mailto:lreaves@smithanderson.com">lreaves@smithanderson.com</a>
<i>Citizens Advisory Council Contact</i>	Northwest CAC Jay Gudeman, 789-9884, <a href="mailto:jay@kilpatrickgudeman.com">jay@kilpatrickgudeman.com</a>

### Case Overview

The site is located on the east side of Creedmoor, in the southeast quadrant of its intersection with Crabtree Valley. Additionally, it is immediately adjacent to Crabtree Valley mall, a regional commercial center at a major crossroads of vehicular and bus transit activity. This rezoning site of 11.74 acres once contained a traditional shopping center and a couple of standalone restaurants, which was demolished in 2004 with plans for redevelopment. A PDD was adopted for the property in 2003. However, with market changes and the PDD and PBOD constraints, the property remained undeveloped since 2003.

### Existing V. Proposed Density/Use Comparison Table

	Existing SC w/PDD & PBOD	Proposed SC & SC CUD w/PBOD
<i>Residential Density</i>	600 DU total	575 DU total
<i>Setbacks</i>	Front – 0 Side – 15 Corner Lot – 30 Rear - 15	Front – 0 Side – 0 Corner Lot – 0 Rear - 0
<i>Max. Building Height</i>	8 stories or 132 feet	120 feet
<i>Office Use</i>	150,000 SF max.	100,000 SF max.
<i>Retail Use</i>	226,000 SF max.	200, 000 SF max.

The property owners are now seeking rezoning for a SC and SC conditional use with a PBOD to facilitate a mixture of complimentary uses on the site. The proposed density, uses, and intensity are lower than that currently permitted and hence will have reduced impacts on infrastructure. With its prime location, easy access, and regional mixed use designation, the site is ideal for redevelopment into a vibrant center containing a mixture of uses and providing a place for people to live, shop, and entertain. The request is consistent with the Future Land Use map and the petitioner addresses the Urban Design Guidelines. Much of the design guidelines are intended to be applied during the site plan review process. The proposed PBOD and streetscape plan meets the intent of Sec. 10-2055 (f)(2) and proposes to enhance a key pedestrian link to the Crabtree Valley mall that will draw pedestrian traffic from the surrounding neighborhoods. The existing site topography and likely future roadway improvements pose challenges for development. The proposal assumes that Crabtree Valley Lane will realign along Edwards Mill Road, thus treating Edwards Mill as the main streetscape frontage. The plan provides for a pedestrian friendly urban project unified by a common streetscape theme.

## **Exhibit C & D Analysis**

### **1. Consistency of the proposed rezoning with the Comprehensive Plan and any applicable City-adopted plan(s)**

#### **1.1 Future Land Use**

The request is consistent with the Future Land Use map. The site is designated as a Regional Mixed use center on the Future Land use map, which are identified as areas to draw major retail and service hubs from across the region. The proposed Shopping Center and conditional use w/PBOD permits a mix of high density housing, office development, hotels, and regional serving retail uses. These uses are encouraged in Regional Mixed use centers,

#### **1.2 Policy Guidance**

The following policy guidance is applicable with this request:

Policy LU 1.3  
Conditional Use District Consistency  
All conditions proposed as part of a conditional use district (CUD) should be consistent with the Comprehensive Plan

The proposed conditions are consistent with the Comprehensive Plan policies.

The following additional Comprehensive Plan policies also apply to this rezoning request:

Policy LU 2.6 - Zoning and Infrastructure Impacts  
Carefully evaluate all amendments to the zoning map that significantly increase permitted density or floor area to ensure that impacts to infrastructure capacity resulting from the projected intensification of development are adequately mitigated or addressed.

The proposed rezoning request seeks to remove an existing PDD and PBOD from the site while applying amended zoning conditions and streetscape standards. The proposed zoning conditions offer reduced residential density and commercial square footage than that permitted under the current zoning. Hence, the proposed rezoning will not impact the infrastructure capacities and is consistent with this policy.

**Policy LU 4.4 Reducing VMT through Mixed Use**

Promote mixed-use development that provides a range of services within a short distance of residences as a way to reduce the growth of vehicle miles traveled.

**Policy LU 4.5 Connectivity**

New development and redevelopment should provide pedestrian and vehicular connectivity between individual development sites to provide alternative means of access along corridors.

The proposed request permits a mix of high density residential uses, retail services, and office development in close proximity to the Crabtree Valley mall, a large regional commercial hub of North Carolina. Though not specifically conditioned for high density residential uses, the streetscape plan conveys the intent for high density residential uses to be developed on the site. The incorporation of the PBOD at this location will enhance a key link to Crabtree Valley mall which will help draw pedestrian traffic from the residential uses on this site as well as the existing neighboring communities to the south. Streetscape features will provide enhanced pedestrian networks and improve connectivity within this area. Thus, the request is consistent with these policies.

**Policy LU 5.1 Reinforcing the Urban Pattern**

New development should be visually integrated with adjacent buildings, and more generally with the surrounding area. Quality design and site planning is required so that new development opportunities within the existing urban fabric of Raleigh are implemented without adverse impacts on local character and appearance.

The proposed request for a mixed use project in close proximity to a regional mall is in keeping with the general character of the area. The proposed streetscape plan attempts to further address street frontages and pedestrian connections to the surrounding areas in a visually cohesive manner with the adoption of compatible streetscape standards. Uniquely challenged for development by the site topographic constraints, the proposal also addresses the impacts of height and scale to the adjacent residential uses. The request is consistent with this policy.

**Policy LU 5.2 Managing Commercial Development Impacts**

Manage new commercial development using zoning regulations and through the conditional use zoning and development review processes so that it does not result in unreasonable and unexpected traffic, parking, litter, shadow, view obstruction, odor, noise, and vibration impacts on surrounding residential areas.

**Policy LU 5.4 Density Transitions**

Low- to medium-density residential development and/or low-impact office uses should serve as transitional densities between lower-density neighborhoods and more intensive commercial and residential uses. Where two areas designated for significantly different development intensity abut on the Future Land Use Map, the implementing zoning should ensure that the appropriate transition occurs on the site with the higher intensity.

**Policy LU 5.5 Transitional and Buffer Zone Districts**

Maintain and enhance zoning districts which serve as transitional or buffer areas between residential and commercial districts and which also may contain institutional, non-profit, and office-type uses. Zoning regulations and conditions for these areas should ensure that development achieves appropriate height and density transitions, and protects neighborhood character.

**Policy LU 5.6 Buffering Requirements**

New development adjacent to areas of lower intensity should provide effective physical buffers to avoid adverse effects. Buffers may include larger setbacks, landscaped or forested strips, transition zones, fencing, screening, height and/or density step downs, and other architectural and site planning measures that avoid potential conflicts.

The proposed request primarily seeks to alter current zoning conditions and reduce the intensity of development already approved for the site, in addition to incorporating more compatible streetscape standards. The request further restricts the height and density currently permitted on the site. The narrow configuration of the site along with the intense grade separations does not lend itself to a tiered or stepped project. The southern border of the property will ultimately be bound by a thoroughfare with a 110 foot right-of-way in the form of re-aligned Crabtree Valley Avenue. Thus, given these existing and future conditions, the site does not lend itself to where a transition and buffer zone can be required. The transitional protective yard requirements prescribed by the City Code Sec. 10-2082.9 will apply and provide for standard buffering required between uses of varying impacts. Based on the uniquely defining site conditions, the request can be considered as meeting the intent of these policies.

**Policy LU 6.4 Bus Stop Dedication**

The city shall coordinate the dedication of land for the construction of bus stop facilities within mixed-use centers on bus lines as part of the development review and zoning process.

Zoning conditions provide for transit easement and shelter options at the time of site plan approval.

**Policy LU 7.1 Encouraging Nodal Development**

Discourage auto-oriented commercial "strip" development and instead encourage pedestrian-oriented "nodes" of commercial development at key locations along major corridors. Zoning and design standards should ensure that the height, mass, and scale of development within nodes respects the integrity and character of surrounding residential areas and does not unreasonably impact them.

**Policy LU 7.4 Scale and Design of New Commercial Uses**

New uses within commercial districts should be developed at a height, mass, scale, and design that is appropriate and compatible with surrounding areas.

**Policy LU 7.6 Pedestrian-Friendly Development**

New commercial developments and redeveloped commercial areas should be pedestrian-friendly.

**Policy LU 10.6 Retail Nodes**

Retail uses should concentrate in mixed-use centers and should not spread along thoroughfares in a linear "strip" pattern unless ancillary to office or high-density residential use.

The proposed request introduces mixed uses within a designated regional mixed use center. The request also seeks to reduce the intensity of uses already approved on the site, while including streetscape standards that further enhance the pedestrian connection to the surrounding uses. Given the site existing conditions and future planned roadway improvements, the site does not lend itself to transitions and buffer zones. The impacts to the surrounding uses are minimal. The proposed reduced maximum height blends in with the terrain to maintain a compatible scale and height. The proposed intensity and scale is appropriate for its mixed use designation.

**Policy LU 8.10 Infill Development**

Encourage infill development on vacant land within the City, particularly in areas where there are vacant lots that create “gaps” in the urban fabric and detract from the character of a commercial or residential street. Such development should complement the established character of the area and should not create sharp changes in the physical development pattern.

**Policy LU 8.11 Development of Vacant Sites**

Facilitate the development of vacant lots that have historically been difficult to develop due to infrastructure or access problems, inadequate lot dimensions, fragmented or absentee ownership, or other constraints. Explore lot consolidation, acquisition, and other measures that would address these.

Located in close proximity to a regional shopping center, this vacant site is well suited to adapt similar high intensity mixed use urban fabric. The streetscape plan proposes to maintain a cohesive pedestrian oriented streetscape edge that connects to the surrounding mall. The request to remove the PDD and PBOD standards will remove restrictive standards that proved unfavorable for developing the property since they were last adopted in 2003. This rezoning will likely open up opportunities for infill development and facilitate development on this vacant lot. The proposed request is consistent with these policies.

**Policy LU 7.5 High-Impact Commercial Uses**

Ensure that the City’s zoning regulations limit the location and proliferation of fast food restaurants, sexually-oriented businesses, late night alcoholic beverage establishments, 24-hour mini-marts and convenience stores, and similar high impact commercial establishments that generate excessive late night activity, noise, or otherwise affect the quality of life in nearby residential neighborhoods.

The proposed zoning conditions prohibit high impact commercial uses and thus, remain consistent with this policy.

**Policy ED 5.3 Creating Attractive Development Sites**

Create attractive and functional sites for new and growing businesses through streetscape improvements and other public realm investments.

The proposal is consistent with this policy. The request provides for streetscape improvements and opportunities to redevelop the site into a mixed use pedestrian friendly development thereby reducing auto-dependence and promoting a livelier pedestrian and transit oriented mixed use center.

**Policy UD 3.5 Visually Cohesive Streetscapes**

Create visually cohesive streetscapes using a variety of techniques including landscaping, undergrounding of utilities, and other streetscape improvements along street frontages that reflect adjacent land uses.

The streetscape plan takes into consideration the realignment of Crabtree Valley Lane and hence, focuses streetscape standards primarily along Edwards Mill Road. Should the realignment not take place, alternate conditions provide for connectivity and uniformity of streetscape elements between the non-PBOD area (Section 3) and the PBOD area (Sections 1 & 2).

**Policy UD 4.3 Improving Streetscape Design**

Improve the appearance and identity of Raleigh's streets through the design of street lights, paved surfaces, landscaped areas, bus shelters, street "furniture," and adjacent building facades.

By integrating streetscape standards, the request remains compatible and connected to the adjacent Crabtree Valley mall. The PBOD enhances a key link to the mall which will help draw pedestrian traffic from this site as well as existing communities to the south. Through the use of site features such as expanded walkways, decorative crosswalks, pedestrian scale lighting and enhanced street furnishings, the project will provide an inviting corridor for pedestrian travel both to and through the project. The request is consistent with policy UD 4.3.

**Policy UD 6.1 Encouraging Pedestrian-Oriented Uses**  
New development, streetscape, and building improvements in Downtown and mixed-use corridors and centers should promote high intensity, pedestrian-oriented use and discourage automobile-oriented uses and drive-through uses.

The proposed rezoning conditions and streetscape plan provide for a mixed use and pedestrian-oriented street network, connecting to the Crabtree Valley mall. Adding increased density at this regional mixed use center allows increased use of pedestrian amenities and provides additional density to support transit as a viable alternative to exclusive auto use. The request is consistent with this policy.

**Policy UD 7.3 Design Guidelines**  
Table UD-1 shall be used to review rezoning petitions and development applications for mixed-use developments or developments in mixed-use areas such as pedestrian Business Overlays, including preliminary site and development plans, petitions for the application of the Pedestrian Business or Downtown overlay districts, Planned Development Districts, and Conditional Use zoning petitions.

Since the majority of the site is located within a designated neighborhood mixed use center, the urban design guidelines apply. The proposed request overall attempts to address each of the urban design guidelines. Several of the guidelines are to be applied during the site planning stage.

*The text below lists each respective Design Guideline, applicants note and related staff comments.*

**Elements of Mixed-Use Areas**

1. *All Mixed-Use Areas should generally provide retail (such as eating establishments, food stores, and banks), office, and residential uses within walking distance of each other.*

**Applicant Response:** The overall project will ultimately provide a mixture of uses, possibly including multi-family, retail and the potential for office uses and/or hospitality.

**Staff Comment:** The proposed rezoning allows for multiple uses and the site is within close proximity to existing high intensity retail, office and residential uses.

**Mixed-Use Areas /Transition to Surrounding Neighborhoods**

2. *Within all Mixed-Use Areas buildings that are adjacent to lower density neighborhoods should transition (height, design, distance and/or landscaping) to the lower heights or be comparable in height and massing.*

**Applicant Response:** The property is bound on the north and east by Crabtree Valley Mall property. To the west is Creedmoor Road and to the southwest is a multi-story assisted care facility on a parcel zoned O & I-1. South across Edwards Mill Road is an area zoned R-6 largely developed for duplex residences. The relationship of this area to the proposed development is such that the finished floor elevation of the area ranges from 15'-30' above the projected finished floor of the of the development proposed in Section 1 of the property. Due to this dramatic grade change, it is likely that only the 3<sup>rd</sup> and 4<sup>th</sup> floors of the building will be visible from Edwards Mill Road. Thus the height of the building will be in keeping with the one and two story residences across the street.

**Staff Comment:** The proposed streetscape plan further reduces the maximum building height to 120' from the existing cap of 132'. The R-6 duplex development to the south of Edwards Mill Road is at a 15-30 foot higher elevation from the subject site. Due to this grade change, visible height would be compatible to existing uses.

### **Mixed-Use Areas /The Block, The Street and The Corridor**

3. *A mixed use area's road network should connect directly into the neighborhood road network of the surrounding community, providing multiple paths for movement to and through the mixed use area. In this way, trips made from the surrounding residential neighborhood(s) to the mixed use area should be possible without requiring travel along a major thoroughfare or arterial.*

**Applicant Response:** All the infrastructure surrounding the property is currently in place to serve this development. At the time of site plan approval, pedestrian connections and/or corridors will be provided to the surrounding streets, allowing for the free flow of pedestrians to and from the site, as adjacent grades permit.

**Staff Comment:** Infrastructure is already in place and future realignment of Crabtree Valley Avenue will change the existing framework. The streetscape plan provides to enhance the Crabtree Valley mall connector road frontage.

4. *Streets should interconnect within a development and with adjoining development. Cul-de-sacs or dead-end streets are generally discouraged except where topographic conditions and/or exterior lot line configurations offer no practical alternatives for connection or through traffic. Street stubs should be provided with development adjacent to open land to provide for future connections. Streets should be planned with due regard to the designated corridors shown on the Thoroughfare Plan.*

**Applicant Response:** All the infrastructure surrounding the property is currently in place to serve this development. At the time of site plan, additional detail will be provided in the form of vehicular and pedestrian connections to the site and adjoining uses.

**Staff Comment:** No new streets are being proposed as part of this development.

5. *Block faces should have a length generally not exceeding 660 feet.*

**Applicant Response:** Specific block configuration will be determined at the time of site plan. In no case will a block length exceed 660 feet without providing either a private street or pedestrian corridor.

**Staff Comment:** This is to be reviewed during site plan review process as no layout details have been provided.

### **Site Design/Building Placement**

6. *A primary task of all urban architecture and landscape design is the physical definition of streets and public spaces as places of shared use. Streets should be lined by buildings rather than parking lots and should provide interest especially for pedestrians. Garage entrances and/or loading areas should be located at the side or rear of a property.*

**Applicant Response:** Along the Edwards Mill Road frontage of the property, specifically Section 1 of the property, we are proposing to line the frontage with the proposed structure and incorporate a wide pedestrian walkway along with enhanced plantings and seating areas. We are proposing a minimum building setback along this frontage of 0' with no off-street parking proposed between the building and the street.

Due to the intense vehicular orientation of Creedmoor Road along with the significant cross slope of the site along this frontage, it is not practical to view this streetscape as a viable place for building facades and pedestrian walkways to interact in a functional manner. It is our intent to create internally focused streetscapes along private drives or pedestrian corridors while inviting pedestrians to interact with the space and the proposed uses in a less vehicular oriented relationship. This level of detail will be provided at the time of site plan approval.

**Staff Comment:** The Edwards Mill Road frontage incorporates a wide sidewalk and minimum building setback, with no parking in between thus promoting an urban environment. Internally focused streetscapes are reasonable given the physical constraints of the site.

7. *Buildings should be located close to the pedestrian street (within 25 feet of the curb), with off-street parking behind and/or beside the buildings.*

**Applicant Response:** At the present time, specific land uses are planned only for Section 1 of the area proposed for rezoning. Within Section 1, the building will be located within 0'-25' of the curb line of the future relocated Crabtree Valley Avenue (aka, Edwards Mill Road). In addition, off-street parking will not be located between the street and the building but rather within internal parking structures. The west portion of the property fronting on Creedmoor Road will limit off-street parking between the building and the street to one double bay of parking. All other parking will be provided internal to the project or within parking structures. This level of detail will be provided at the time of site plan approval.

**Staff Comment:** The Edwards Mill Road frontage places buildings closer to the street and prohibits parking in between. This is the primary frontage for applying the streetscape standards.

8. *If the building is located at a street intersection, the main building or part of the building placed should be placed at the corner. Parking, loading or service should not be located at an intersection.*

**Applicant Response:** We will be in compliance with this requirement. This level of detail will be shown at the time of site plan approval.

**Staff Comment:** This is to be reviewed during site plan review process as no layout details have been provided.

#### **Site Design/Urban Open Space**

9. *To ensure that urban open space is well-used, it is essential to locate and design it carefully. The space should be located where it is visible and easily accessible from*

*public areas (building entrances, sidewalks). Take views and sun exposure into account as well.*

**Applicant Response:** We intend to provide a variety of open space opportunities in the form of linear parks along the frontages that will be open to the public, as well as some internal private gardens for Section 1 of the property to be used by the residents of the development. This level of detail will be shown at the time of site plan approval.

**Staff Comment:** This is to be reviewed during site plan review process as site design details have not been provided.

*10. New urban spaces should contain direct access from the adjacent streets. They should be open along the adjacent sidewalks and allow for multiple points of entry. They should also be visually permeable from the sidewalk, allowing passersby to see directly into the space.*

**Applicant Response:** See pages 5-10 of the Streetscape and Parking Plan for conceptual images of our perimeter urban spaces for Sections 1 and 2 of the area proposed for rezoning. Additional detail will be shown at the time of site plan approval.

**Staff Comment:** Conceptual open space images are provided with additional details due at the site plan review time.

*11. The perimeter of urban open spaces should consist of active uses that provide pedestrian traffic for the space including retail, cafés, and restaurants and higher-density residential.*

**Applicant Response:** Once the three Sections of the property (as identified in the Zoning Exhibit) are site planned and defined, the relationship between the components can be refined to include these types of active spaces. This level of detail will be defined at the time of site plan approval.

**Staff Comment:** This is to be reviewed during site plan review process as conditions do not specify the placement of uses with respect to open space.

*12. A properly defined urban open space is visually enclosed by the fronting of buildings to create an outdoor "room" that is comfortable to users.*

**Applicant Response:** Once the three Sections of the property (as identified in the Zoning Exhibit) are site planned and defined, the relationship between the components can be refined to include these types of active spaces. This level of detail will be defined at the time of site plan approval.

**Staff Comment:** This is to be reviewed during site plan review process as site design details have not been provided.

#### **Site Design/Public Seating**

*13. New public spaces should provide seating opportunities.*

**Applicant Response:** As identified in the Streetscape and Parking Plan we are proposing multiple seating opportunities along the south and east frontages of the project for Sections 1 and 2 of the property. Additional details will be provided at the time of site plan approval.

**Staff Comment:** The Streetscape plan provides for seating along the south and east frontages.

### **Site Design/Automobile Parking and Parking Structures**

14. *Parking lots should not dominate the frontage of pedestrian-oriented streets, interrupt pedestrian routes, or negatively impact surrounding developments.*

**Staff Comment:** Per the streetscape plan provisions, parking is to be provided internally.

15. *Parking lots should be located behind or in the interior of a block whenever possible. Parking lots should not occupy more than 1/3 of the frontage of the adjacent building or not more than 64 feet, whichever is less.*

**Staff Comment:** Per the streetscape plan provisions, parking is to be provided internally. Further details to be reviewed during site plan review process.

16. *Parking structures are clearly an important and necessary element of the overall urban infrastructure but, given their utilitarian elements, can give serious negative visual effects. New structures should merit the same level of materials and finishes as that a principal building would, care in the use of basic design elements can make a significant improvement.*

**Applicant Response 14-16:** Within Section 1 of the area proposed for rezoning, no off-street parking will be located between the street and the building but rather within internal parking structures. The west portion of the property fronting on Creedmoor Road will limit off-street parking between the building and the street to one double bay of parking. All other parking will be located internal to the project or within parking structures. This level of detail will be illustrated at the time of site plan approval.

**Staff Comment:** Edwards Mill frontages specify parking to the rear. Further details to be reviewed during site plan review process.

### **Site Design/Transit Stops**

17. *Higher building densities and more intensive land uses should be within walking distance of transit stops, permitting public transit to become a viable alternative to the automobile.*

**Staff Comment:** The site is adjacent to transit on both Edwards Mill and Creedmoor Road and a transfer station close to the mall.

18. *Convenient, comfortable pedestrian access between the transit stop and the building entrance should be planned as part of the overall pedestrian network.*

**Applicant Response 17-18:** The site is located immediately adjacent to transit on both Edwards Mill and Creedmoor Road. In addition, there is a bus transfer station located within walking distance of the site within the parking structure of Crabtree Valley Mall.

**Staff Comment:** This is to be reviewed during site plan review process as site layout details have not been provided.

### **Site Design/Environmental Protection**

19. *All development should respect natural resources as an essential component of the human environment. The most sensitive landscape areas, both environmentally and visually, are steep slopes greater than 15 percent, watercourses, and floodplains. Any development in these areas should minimize intervention and maintain the natural condition except under extreme circumstances. Where practical, these features should be conserved as open space amenities and incorporated in the overall site design.*

**Applicant Response:** This property will be the redevelopment of an old shopping center. As such, there are no sensitive areas on the property. The limits of development will be defined at the time of site plan.

**Staff Comment:** The plan proposes height limits that blend in with terrain changes thus respecting the environmental features.

**Street Design/General Street Design Principles**

20. *It is the intent of these guidelines to build streets that are integral components of community design. Streets should be designed as the main public spaces of the City and should be scaled for pedestrians.*

**Staff Comment:** Streetscape plan addresses pedestrian scale along streets where the PBOD's apply.

21. *Sidewalks should be 5-8 feet wide in residential areas and located on both sides of the street. Sidewalks in commercial areas and Pedestrian Business Overlays should be a minimum of 14-18 feet wide to accommodate sidewalk uses such as vendors, merchandising and outdoor seating.*

**Applicant Response 20-21:** It is our intent to comply with these guidelines as illustrated in our Streetscape and Parking plan. Final details will be shown at the time of site plan approval.

**Staff Comment:** Streetscape plan addresses pedestrian scale along streets where the PBOD's apply.

22. *Streets should be designed with street trees planted in a manner appropriate to their function. Commercial streets should have trees which compliment the face of the buildings and which shade the sidewalk. Residential streets should provide for an appropriate canopy, which shadows both the street and sidewalk, and serves as a visual buffer between the street and the home. The typical width of the street landscape strip is 6-8 feet. This width ensures healthy street trees, precludes tree roots from breaking the sidewalk, and provides adequate pedestrian buffering. Street trees should be at least 6 1/4" caliper and should be consistent with the City's landscaping, lighting and street sight distance requirements.*

**Applicant Response:** A detailed landscape plan will be provided at the time of site plan. Street trees will be installed at a minimum of 3" caliper in order to assure their survival and give them the best chance at adapting to the urban environment.

**Staff Comment:** The Streetscape Plan includes detailed sidewalk and planting design standards that will aid in the growth of healthy street trees. A landscape plan will be required during site plan review process.

**Street Design/Spatial Definition**

23. *Buildings should define the streets spatially. Proper spatial definition should be achieved with buildings or other architectural elements (including certain tree plantings) that make up the street edges aligned in a disciplined manner with an appropriate ratio of height to width.*

**Applicant Response:** This level of detail will be illustrated at the time of site plan.

**Staff Comment:** The Edwards Mill Road frontage places buildings closer to the street and prohibits parking in between. Further details to be evaluated during site plan review process.

***Building Design/Facade Treatment***

24. *The primary entrance should be both architecturally and functionally on the front facade of any building facing the primary public street. Such entrances shall be designed to convey their prominence on the fronting facade.*

**Staff Comment:** This is to be reviewed during the site plan review process as site plan details have not been provided.

25. *The ground level of the building should offer pedestrian interest along sidewalks. This includes windows entrances, and architectural details. Signage, awnings, and ornamentation are encouraged.*

**Applicant Response 24-25:** This level of detail will be illustrated at the time of site plan.

**Staff Comment:** This is to be reviewed during the site plan review process as building and architectural details have not been provided.

***Building Design/Street Level Activity***

26. *The sidewalks should be the principal place of pedestrian movement and casual social interaction. Designs and uses should be complementary to that function.*

**Applicant Response:** This level of detail will be illustrated at the time of site plan.

**Staff Comment:** This is to be reviewed during the site plan review process as site plan details have not been provided.

**1.3 Area Plan Guidance**

The site falls within the limits of the Crabtree SAP. The plan's goal is for the area to develop as a mixed-use environment with people living, working, and shopping within a walkable urban community. The following additional policies apply:

<p>Policy AP-C 2 Crabtree Area Hillsides Hillsides in the Crabtree area should be retained and not graded down for incongruous, large footprint buildings. New structures on hillsides and hilltops should fit into the terrain.</p> <p>Policy AP-C 4 Crabtree Mall Connections A two level circulation system is proposed for the Crabtree Mall area. The lower level, which corresponds to the lower level of the Mall, should include vehicular, transit, and pedestrian circulation (including greenways). The upper circulation level corresponds to the upper level of the Mall but ground level of Kidds Hill Plaza and the hotel areas to the north and east of the Mall and requires that pedestrian bridges be accommodated as sites adjacent to the Mall are developed.</p>
--

The proposed request respects the terrain of the site and seeks to blend the development into the hillside. The streetscape plan provides for an enhanced connector road between Edwards Mill Road and Crabtree Valley mall, thus connecting the Kidds Hill Plaza to the mall. The request is consistent with these policies.

**Policy AP-C 5 Design Unity in the Crabtree Area**

Where possible, contiguous tracts throughout the Crabtree area should have some sense of overall design unity. In areas where upper-level pedestrian access is developed, creative structural expression of these circulation elements would serve to engage the users and give the area character. The use of glass and transparent materials should be encouraged in order to keep the area from appearing closed in, like a tunnel.

The proposed streetscape plan integrates site features such as expanded walkways, decorative crosswalks, pedestrian scale lighting and enhanced street furnishings along the frontages to provide an inviting corridor for pedestrian travel both to and through the project. This promotes a unified design element for the corridor. Alternate conditions provide for connectivity and uniformity of streetscape elements between the non-PBOD area (Section 3) and the PBOD area (Sections 1 & 2).

**2. Compatibility of the proposed rezoning with the property and surrounding area**

The site is located in a prominent location, adjacent to Crabtree Valley mall and at the crossroads of two major thoroughfares, making it a prime candidate for redevelopment into a vibrant center containing a mix of uses and providing a place for people to live, shop, and entertain. The area is surrounded predominantly by a mix of high intensity commercial and office uses, with low and medium density residential uses to the south and southwest. The property is currently subject to a Planned Development District with a PBOD and a detailed streetscape plan adopted in 2003. The property to date remains undeveloped and with market changes, the constraints of the current PDD have proven impractical to abide by and the current owner is seeking alternate zoning through the implementation of a Shopping Center and conditional use with an amended PBOD to facilitate a mix of complimentary uses on the site.

The proposed request seeks to reduce the currently approved intensity on the site. While the amended Streetscape plan eliminates the detailed specifications that applies currently, it includes alternative streetscape standards that meet the requirements of Sec.10-2055 (f)(2). The streetscape and public spaces are intended to support the urban nature this site offers. The incorporation of the PBOD at this location enhances a key link to the Crabtree Valley mall which will help draw pedestrian traffic from the residential at Crabtree Place as well as existing neighboring communities to the south. Through the use of site features such as expanded walkways, decorative crosswalks, pedestrian scale lighting and enhanced street furnishings, the project will provide an inviting corridor for pedestrian travel both to and through the project. Additionally, the building height maximums are further reduced and impacts to residential uses mitigated by the terrain constraints. Thus, with reduced density and added streetscape and pedestrian networking, the proposed rezoning appears to be compatible to the surrounding uses and zoning.

**3. Public benefits of the proposed rezoning**

The proposed rezoning benefits immediate neighbors by facilitating development of currently vacant property for a mix of uses at densities less than those authorized by current zoning. The amendment does not materially change what can be developed on the property. While the proposal reduces the height and intensities of mixed uses that can be developed on the site, it provides for a better transition to the surrounding low density residential uses. It additionally helps enhance a key link to Crabtree Valley mall which will help draw pedestrian traffic from existing neighboring communities, thus meeting the intent of several policies stated in the Crabtree SAP.

#### 4. Detriments of the proposed rezoning

Staff assessment indicates the following potential detriments:

- A discontinuous streetscape edge along Crabtree Valley, closer to its intersection with Creedmoor Road created from the removal of existing PBOD for a small section closer to the Creedmoor Crabtree Valley intersection (*applicant reasons this to grade issues and future potential realignment of Crabtree Valley*)
- Lesser predictability of site layout and design elements due to removal of Planned Development District and associated Master plan standards (*applicant notes that the existing PDD and PBOD place overly restraining constraints under the current market conditions due to which property has remained undeveloped since 2003*)
- Reduced height and density in designated regional centers intended for higher intensities (*applicant notes that the proposed reduced intensities at this site provides a better transition to surrounding low density residential uses*)

#### 5. The impact on public services, facilities, infrastructure, fire and safety, parks and recreation, etc.

##### 5.1 Transportation

Primary Streets	Classification	2010 COR Estimated Traffic Volume (ADT)				
Creedmoor Road	Secondary Arterial	21,000				
Crabtree Valley Avenue	Major Thoroughfare	5,500				
Edwards Mill Road	Commercial	N/A				
Street Conditions						
Creedmoor Road	Lanes	Street Width	Curb and Gutter	Right-of-Way	Sidewalks	Bicycle Accommodations
Existing	5	80'	Back-to-back curb and gutter section	100'	5' sidewalks on west side	None
City Standard	6	89'	Back-to-back curb and gutter section	110'	minimum 5' sidewalks on both sides	Striped bicycle lanes on both sides
Meets City Standard?	NO	NO	YES	NO	YES	NO
Crabtree Valley Avenue	Lanes	Street Width	Curb and Gutter	Right-of-Way	Sidewalks	Bicycle Accommodations
Existing	3	41'	Back-to-back curb and gutter section	80'	12' Multi-use path on east side	None
City Standard	4	65'	Back-to-back curb and gutter section	90'	minimum 14' sidewalks on both sides	Striped bicycle lanes on both sides
Meets City Standard?	NO	NO	YES	NO	NO	NO
Edwards Mill Road	Lanes	Street Width	Curb and Gutter	Right-of-Way	Sidewalks	Bicycle Accommodations
Existing	2	20'	None	60'	None	None
City Standard	2	41'	Back-to-back curb and gutter section	60'	minimum 14' sidewalks on both sides	None
Meets City Standard?	YES	NO	NO	YES	NO	N/A
Expected Traffic Generation [vph]	Current Zoning	Proposed Zoning	Differential			
AM PEAK	774	705	-69			

PM PEAK	1076	934	-142			
<b>Suggested Conditions/ Impact Mitigation:</b>			Traffic Study Determination: Staff has calculated a trip generation differential for this case. The applicant has agreed to limit build-out of the site and has provided a "trip budget" of 12,947 daily trips, 705 AM peak trips and 934 PM peak trips.			
<b>Additional Information:</b>	Neither NCDOT nor the City of Raleigh have any roadway construction projects scheduled in the vicinity of this case.					

Impact Identified: Development of the subject property will add additional traffic to an area that, at present, is congested during peak periods.

Proposed Mitigation: Conditions address traffic impacts by limiting land uses to the traffic capacities supported by the existing infrastructure.

### 5.2 Transit

Please dedicate 15x20' transit easements on Creedmoor Rd, Crabtree Valley Ave and Edwards Mill Rd. Concurrent with building construction please erect an ADA accessible transit waiting shelter that is compatible with the street furnishings prescribed for the Crabtree Place Streetscape on the easements on Crabtree Valley Ave and Edwards Mill Rd.

The development of this property is likely to significantly increase transit ridership. As people live, work and shop in Crabtree Place the pedestrian scale will encourage them to use transit. Furthermore, the Crabtree Valley Pedestrian Circulation Plan aims to increase pedestrian mobility which in turn increases transit use.

Impact Identified: Transit easement and shelter options are provided for in the zoning conditions and will be implemented during the site plan approval process.

### 5.3 Hydrology

<i>Floodplain</i>	Site is located just outside the limits of FEMA floodplain.
<i>Drainage Basin</i>	Crabtree
<i>Stormwater Management</i>	Subject to Part 10, Chapter 9
<i>Overlay District</i>	None

Impact Identified: Site is subject to Part 10, Chapter 9 Stormwater regulations. No floodplain or Neuse River Buffers located on site.

### 5.4 Public Utilities

	<i>Maximum Demand (current)</i>	<i>Maximum Demand (proposed)</i>
<i>Water</i>	52,830 GPD	52,830 GPD
<i>Waste Water</i>	52,830 GPD	52,830 GPD

The proposed rezoning would not increase flows to the City's wastewater collection or water distribution systems. There are existing public sanitary sewer and water mains in the adjacent rights-of way surrounding the zoning area.

Impact Identified: None

### **5.5 Parks and Recreation**

1. The existing Crabtree Creek Trail is located north of Crabtree Valley Avenue. It is recommended that the applicant provide pedestrian access to this existing trail.
2. There are no park search areas in the location.

Impact Identified: There are no impacts to recreation service delivery.

### **5.6 Urban Forestry**

A buy-out fee was collected for the tree conservation areas required on PIN 0796308642.

Impact Identified: No additional impacts identified based upon the revised conditions (dated 6-2-11).

### **5.7 Wake County Public Schools**

The proposed request reduces the residential density from the current 627 DU to 575 DU thereby reducing current impacts to the school.

Impact Identified: None

### **5.8 Designated Historic Resources**

There are no historic resources located on the site.

Impact Identified: None.

### **5.9 Impacts Summary**

- It is recommended that the applicant provide pedestrian access to the Crabtree Creek trail to the north.

### **5.10 Mitigation of Impacts**

- Conditions address traffic impacts by limiting land uses to the traffic capacities supported by the existing infrastructure.

## **6. Appearance Commission**

The site is being proposed for a Pedestrian Business Overlay District and is hence subject to review by the Appearance Commission. AC reviewed this request on June 21, 2011. The comments are noted below:

1. The committee recommends that, in keeping with Sec. 10-2055(f)(2)e. & k., the Streetscape Plan include details as to how connectivity between the site and City of Raleigh Greenway system will be provided.

## **7. Conclusions**

The subject request is consistent with its Future Land Use map designation and with several other key Comprehensive Plan policies. Given its location within a designated regional mixed use center, the proposed mixed uses along with streetscape plan standards promotes a vibrant, pedestrian friendly urban project in close proximity to Crabtree Valley mall, the largest commercial center in the area. Uniquely challenged by

existing topographical constraints along with future planned right-of-way improvements, it is appropriate to consider application of urban design guidelines during the site planning process. By removing a Planned Development District, amending PBOD standards, and proposing lower densities, the request provides for a more appropriate transition to the surrounding low density residential uses and improved pedestrian connectivity to the mall, thus furthering the goals and policies of the Crabtree SAP.

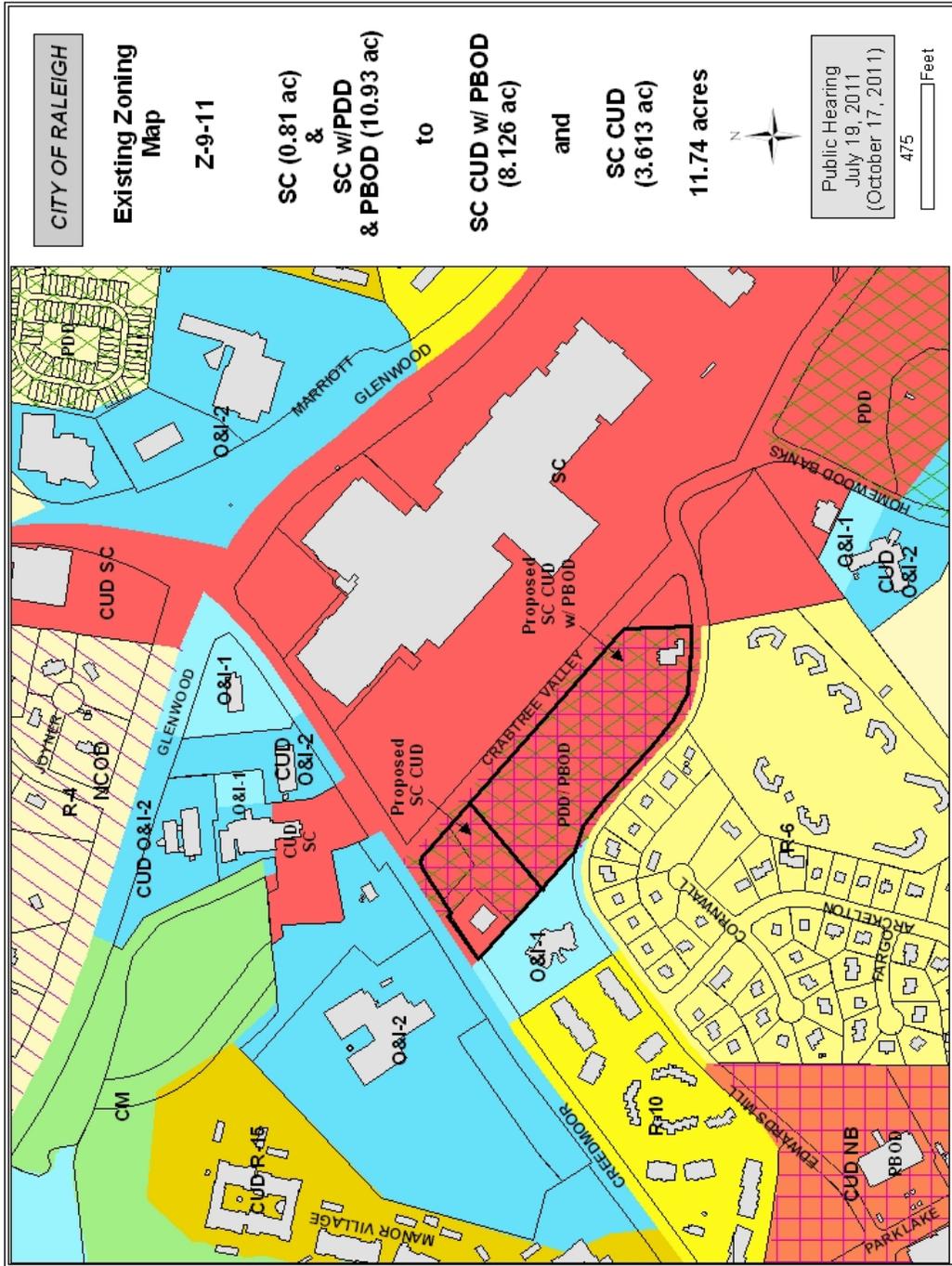
**Outstanding Issues:**

- Staff comments on Streetscape Plan (Deferred by applicant to site plan review process. See attached memo).

**Streetscape Plan Comments**

- Brick banding and concrete scoring pattern on sidewalk should be laid out in SSP, not at site plan approval.
- Curbside planters need dimensions and spacing.
- Building edge should be softened by ground floor use and architectural treatments like windows, doors, materials etc. Building side planters are not necessary as they seem to narrow down the sidewalk space.
- Signage: limit medium and high profile ground signs.
- Add language about how parking will be screened- number of plants, height, parking deck screens, placement X feet behind sidewalk min, no parking on the ground floor of buildings etc.

# Existing Zoning Map





SMITH, ANDERSON, BLOUNT,  
DORSETT, MITCHELL & JERNIGAN, L.L.P.

OFFICES  
2500 Wachovia Capitol Center  
Raleigh, North Carolina 27601

LAWYERS

MAILING ADDRESS  
P.O. Box 2611  
Raleigh, North Carolina  
27602-2611

August 4, 2011

LACY H. REAVES  
DIRECT DIAL: (919) 821-6704  
E-Mail: lreaves@smithlaw.com

TELEPHONE: (919) 821-1220  
FACSIMILE: (919) 821-6800

Via E-Mail

Ms. Dhanya Sandeep  
Department of City Planning  
City of Raleigh  
One Exchange Plaza  
Suite 304  
Raleigh, North Carolina 27602-590

Re: Zoning Case Z-9-11 – Streetscape and Parking Plan

Dear Dhanya:

As requested in your message this morning, we provide the following responses to your questions with regard to the Streetscape Plan in this case:

Streetscape Plan Comments

- **Brick Banding and Concrete Scoring Pattern on Sidewalks**

Response: The design of the banding and scoring on the sidewalks will be coordinated with the design of the buildings to achieve overall unity of development. Because the buildings and their façades have not yet been designed, we are not in a position to provide specificity in the Streetscape Plan with regard to the brick banding and concrete scoring pattern on the sidewalks. We do not want to provide design detail at this time that is uncoordinated with building and façade designs.

- **Dimension and Spacing of Curbside Planters**

Response: The spacing and dimensions of the curbside planters, which are optional streetscape features, will depend upon the placement and number of parallel parking spaces. Planter dimensions and spacing will also be coordinated with building façade designs. Until these matters are addressed later in the design process, we cannot provide additional information concerning the curbside planters.

Ms. Dhanya Sandeep

August 4, 2011

Page 2

- **Softening of Building Edge**

Response: As noted at page 15 of the Streetscape Plan, ground level residential units within this multi-family residential development will exhibit at least one of the following elements: balconies, porches, stoops or similar active elements. The design process for the development has not yet progressed to the point that we can provide detail with regard to the building façades.

- **Signage**

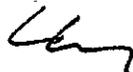
Response: Signage within the PBOD will be provided in accordance with standards established by the City's ordinances. Sign colors and design will be coordinated with other design elements of the development when they are established.

- **Screening of Parking**

Response: The design process for this development has not advanced to a stage that allows us to address specifics of parking screening in the Streetscape Plan.

I hope that the foregoing is helpful and know that Staff understands the limitations attendant with the logical progression of the design process. We look forward to discussing this case with the Planning Commission.

Very truly yours,



Lacy H. Reaves

LHR/lj



# RALEIGH APPEARANCE COMMISSION

Planning Department · PO Box 590 · Raleigh, North Carolina 27602

## MEMORANDUM

TO: City Council  
FROM: Raleigh Appearance Commission  
RE: Rezoning case Z-9-11  
DATE: July 19, 2011

As per the provisions of North Carolina General Statutes §160A-452 and City of Raleigh Code of Ordinances §10-1021 and §10-2055, on June 21, 2011 the Raleigh Appearance Commission reviewed rezoning proposal Z-9-11.

At the meeting, representatives of the case offered an overview of the proposal, with discussion following. At the conclusion of commission's discussion, the commission moved by acclimation that the applicants consider amending their proposal to include the following additional provision:

1. That, as per Code Sec. 10-2055(f)(2)e. & k., details be provided in the Streetscape Plan as to how connectivity between the site and City of Raleigh Greenway system will be accomplished.

Thank you for the opportunity to provide this comment.

For the Raleigh Appearance Commission,

Handwritten signature of Vincent Whitehurst in black ink.

Vincent Whitehurst, Chair

Handwritten signature of Ted Van Dyk in black ink.

Ted Van Dyk, Vice-Chair



# Petition to Amend the Official Zoning Map

Before the City Council of the City of Raleigh, North Carolina

Z-9-11  
3-18-11

pd 1064.<sup>00</sup>  
by CK#  
1659

The following items are required with the submittal of rezoning petition. For additional information on these submittal requirements, see the *Filing Instructions* addendum.

## Rezoning Application Submittal Package Checklist

- Completed Rezoning Application which includes the following sections:
  - Signatory Page
  - Exhibit B
  - Exhibit C (only for Conditional Use filing)
  - Exhibit D
  - Map showing adjacent property owner names with PIN's
- Application Fee
  - \$532 for General Use Cases
  - \$1064 for Conditional Use Cases
  - \$2659 for PDD Master Plans
- Neighborhood Meeting Report (only for Conditional Use filing)
- Receipt/ Verification for Meeting Notification Mail out
- Traffic Impact Generation Report OR written waiver of trip generation from Raleigh Transportation Services Division
- (General Use ONLY) if applicant is not the petitioner must provide proof of notification to the adjacent property owners per G.S. 160A-384

2011 MAR 18 AM 9:40

CITY OF RALEIGH  
CITY PLANNING DEPT



CITY OF RALEIGH  
CITY PLANNING DEPT

# Petition to Amend the Official Zoning Map

Before the City Council of the City of Raleigh, North Carolina

2-9-11  
3-18-11

2011 MAR 8 AM 9:10  
CITY OF RALEIGH  
CITY PLANNING DEPT

The petitioner seeks to show the following:

1. That, for the purposes of promoting health, morals, or the general welfare, the zoning classification of the property described herein must be changed.
2. That the following circumstance(s) exist(s):
  - City Council has erred in establishing the current zoning classification of the property by disregarding one or a combination of the fundamental principles of zoning as set forth in the enabling legislation, North Carolina General Statutes Section 160A-381 and 160A-383.
  - Circumstances have so changed since the property was last zoned that its current zoning classification could not properly be applied to it now were it being zoned for the first time.
  - The property has not heretofore been subject to the zoning regulations of the City of Raleigh.
3. That the requested zoning change is or will be consistent with the Raleigh Comprehensive Plan.
4. That the fundamental purposes of zoning as set forth in the N.C. enabling legislation would be best served by changing the zoning classification of the property. Among the fundamental purposes of zoning are:
  - a. to lessen congestion in the streets;
  - b. to provide adequate light and air;
  - c. to prevent the overcrowding of land;
  - d. to facilitate the adequate provision of transportation, water, sewerage, schools, parks, and other public requirements;
  - e. to regulate in accordance with a comprehensive plan;
  - f. to avoid spot zoning; and
  - g. to regulate with reasonable consideration to the character of the district, the suitability of the land for particular uses, the conservation of the value of buildings within the district and the encouragement of the most appropriate use of the land throughout the City.

THEREFORE, petitioner requests that the Official Zoning map be amended to change the zoning classification of the property as proposed in this submittal, and for such other action as may be deemed appropriate. All property owners must sign below for conditional use requests.

**ALL CONDITIONAL PAGES MUST BE SIGNED BY ALL PROPERTY OWNERS**

Signature(s)  
Weingarten/Investments, Inc.

By: Alan R. Koford

Print Name and Title: **Alan R. Koford**  
**Sr. Vice President**

Date: 15 MARCH 2011

# EXHIBIT B. Request for Zoning Change

Please use this form only – form may be photocopied. Please type or print. See instructions in *Filing Addendum*

## Contact Information

	Name(s)	Address	Telephone/Email
Petitioner(s)	Weingarten Investments Inc.	9420 Forum Drive, Ste. 101 Raleigh, NC 27615	(919) 845-3950 <a href="mailto:mkaney@weingarten.com">mkaney@weingarten.com</a>

(for conditional use requests, petitioners must own petitioned property)

Property Owner(s) Same as Above

Contact Person(s)	Lacy H. Reaves	2500 Wachovia Capital Center P.O. Box 2611 Raleigh, NC 27602	(919) 821-6704 <a href="mailto:lreaves@smithanderson.com">lreaves@smithanderson.com</a>
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## Property information

Property Description (Wake County PIN)	0796-30-2996, 0796-30-8642, 0796-31-4150
---	--

Nearest Major Intersection	Southeast corner of Creedmoor Road and Crabtree Valley Avenue.
----------------------------	--

Area of Subject Property (in acres)	+/- 11.74
--	-----------

Current Zoning Districts (include all overlay districts)	Shopping Center District and Shopping Center District with Planned Development Conditional Use Overlay District and Pedestrian Business Overlay District.
---	---

Requested Zoning Districts (include all overlay districts)	<u>Approximately 8.126 acres:</u> Shopping Center Conditional Use District with Pedestrian Business Overlay District. This area is shown as Section 1 and Section 2 on Exhibit B-1 attached hereto. <u>Approximately 3.613 acres:</u> Shopping Center Conditional Use District. This area is shown as Section 3 on Exhibit B-1 attached hereto.
---	--

# EXHIBIT B. Request for Zoning Change

Please use this form only -- form may be photocopied. Please type or print. See instructions in Filing Addendum

The following are all of the persons, firms, property owners, associations, corporations, entities or governments owning property adjacent to and within one hundred feet (excluding right-of-way) of the property sought to be rezoned. Please include Wake County PINs with names, addresses and zip codes. Indicate if property is owned by a condominium property owners association. Please complete ownership information in the boxes below. If you need additional space, please copy this form.

Name	Street Address	City/State/Zip	Wake Co. PIN
CNL RETIREMENT SUN1 LP HCP SUN1	DELOITTE TAX LLP ATTN: HCPI 2235 FARADAY AVE STE O	CARLSBAD CA 92008-7215	0796301789
PINNACLE APARTMENTS LLC	DRUCKER & FALK 11824 FISHING POINT DR	NEWPORT NEWS VA 23606-2679	0795399646
WILLIAMS, JAMES	5004 EDWARDS MILL RD	RALEIGH NC 27612-4420	0796309117
LEWIS, MICHAEL W & JEANETTE S	8117 PONY PASTURE CT	RALEIGH NC 27612-7379	0796308224
CVM ASSOCIATES	PO BOX 31147	RALEIGH NC 27622-1147	0795497805
LANGLEY, VICTORIA M MARTIN, SIDNEY A	717 BLENHHEIM DR	RALEIGH NC 27612-4902	0796312398
CVM HOLDINGS LLC	PO BOX 31147	RALEIGH NC 27622-1147	0796502569
GRANTHAM, CHARLES E	3909 RHODODENDRON DR	RALEIGH NC 27612-5430	0796303474
NATIONWIDE MUTUAL INSURANCE COMPANY	ONE NATIONWIDE PLAZA MAILCODE 1-03-304	COLUMBUS OH 43215	0796218350
GRANTHAM, CHARLES EDWARD GRANTHAM, SHERRI POWELL	3909 RHODODENDRON DR	RALEIGH NC 27612-5430	0796305476
CVM HOLDINGS LLC	PO BOX 31147	RALEIGH NC 27622-1147	0796404276
GRANTHAM, CHARLES E	3909 RHODODENDRON DR	RALEIGH NC 27612-5430	0796304477
GRANTHAM, CHARLES EDWARD	3909 RHODODENDRON DR	RALEIGH NC 27612-5430	0796307332
N C DEPARTMENT OF TRANSPORTATION	PO BOX 15580	DURHAM NC 27704-0580	0796408142
REESE, WILLIAM E II & JENNIFER S	5300 LAKE EDGE DR	HOLLY SPRINGS NC 27540-9339	0796306359

2-9-11

# EXHIBIT C. Request for Zoning Change

Please use this form only – form may be photocopied. Please type or print. See instructions in *Filing Addendum*

Z-9-11  
8/15/11  
pg 1 of 2

**1) Conditional Use District requested:** Shopping Center Conditional Use District with Pedestrian Business Overlay District (portion) and Shopping Center Conditional Use District (portion)

**2) Narrative of conditions being requested:**

For the purposes of the following conditions, "Property" shall refer to tax parcels (PINs) 0796-30-2996, 0796-30-8642, and 0796-31-4150.

- (a) The following uses shall be prohibited upon the Property: automotive service and repair facility; utility service and substation; emergency shelter Type A or Type B; landfill; manufacturing-custom; manufacturing-specialized; adult establishment; kennel/cattery; riding stable; correctional/penal facility; and outdoor stadium, outdoor theater or outdoor racetrack.
- (b) With the exception of Hotel/Motel, an allowed use upon the Property, all uses developed upon the Property that are categorized as "Commercial" in the Schedule of Permitted Land Uses attached hereto as Exhibit C-1 shall not exceed 200,000 square feet floor area gross.
- (c) All uses developed upon the Property that are characterized as Institution/Civic/Services or Office in the Schedule of Permitted Land Uses attached hereto as Exhibit C-1 shall not exceed 100,000 square feet floor area gross.
- (d) Uses developed upon the Property that are characterized as Residential in the Schedule of Permitted Land Uses attached hereto as Exhibit C-1 shall not exceed 575 dwelling units.
- (e) Within fifteen (15) days following the approval of the form and substance of the restrictive covenant hereinafter mentioned by the City Attorney or his or her deputy, the owner of the Property shall cause to be recorded in the Wake County Registry a restrictive covenant that allocates allowable development upon the Property as provided in the foregoing conditions (b), (c), and (d) among all existing lots of record comprising the Property. Such restrictive covenant shall be submitted to the City Attorney within thirty (30) days following approval of this zoning case by the City Council and shall be approved by the City Attorney or his or her deputy prior to recordation. Such restrictive covenant shall provide that it shall become null and void and of no effect whatsoever in the event that a court enters a final judgment (not subject to appeal) declaring the rezoning resulting from the approval of this case invalid. The restrictive covenant and the allocation of development set forth therein may be amended from time to time following

I acknowledge that these restrictions and conditions are offered voluntarily and with knowledge of the guidelines stated in the *Filing Addendum*. If additional space is needed, this form may be copied. Each page must be signed by all property owners.

**ALL CONDITIONAL PAGES MUST BE SIGNED BY ALL PROPERTY OWNERS**

Signature(s)  
Weingarten Investments Inc.

By: William E Coats

Print Name and Title: William E Coats, Regional Development Director  
8/15/11

# EXHIBIT C. Request for Zoning Change

Please use this form only – form may be photocopied. Please type or print. See instructions in *Filing Addendum*

2-9-11  
8/15/11  
pg 2 of 2

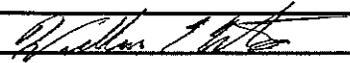
recording. Any such amendment shall require the written concurrence of the owner(s) of all portions of the Property affected by the amendment and shall be at the sole discretion of such owner(s). Following recording a copy of each amendment shall be mailed to the Planning Director at P.O. Box 590, Raleigh, N.C. 27602.

- (f) The land use or uses developed upon the Property shall be limited to a use or uses which when analyzed using the Trip Generation Manual published by the Institute of Transportation Engineers will generate no more than 12,947 daily vehicle trips with no more than 705 vehicle trips in the AM peak hour and no more than 934 vehicle trips in the PM peak hour.
- (g) Upon development of Section 1 of the Property as shown on Exhibit B-1 of the Petition in this case, a means shall be provided for pedestrian connectivity from the then existing grade of Section 1 to the grade of Crabtree Valley Avenue as it is now located between the Property and Crabtree Valley Mall (PIN 0796-50-2569).
- (h) The actual location of the "Crabtree Valley Mall Connector Road" shown on pages 4 and 16 of the Streetscape and Parking Plan in this case will be established at the time of site plan approval.
- (i) Along the Creedmoor Road frontage of the Property, parking between the street right-of-way and the building(s) shall not exceed two rows of parking.
- (j) Upon approval of a site plan or plans for the development of all or a portion of the Property, if requested by the City's Transit Division, the owner shall dedicate a transit easement approved by the Raleigh City Attorney upon the respective site or sites of a size, nature and location acceptable to such Division and provide any improvements, such as a bench and/or shelter, specified by such Division that shall be in accordance with its standard policies. The owner shall dedicate up to three (3) transit easements upon the Property.
- (k) Upon development of Section 3 of the Property as shown on Exhibit B-1 of the Petition in this case, pedestrian connectivity shall be provided between such Section and the adjoining Section 2. With regard to sidewalks along public rights of way, the design of sidewalk sections and curb and sidewalk details for Section 3 shall be coordinated with such design for Sections 1 and 2, although to the extent allowed by the City's Ordinances, sidewalk widths may be reduced for Section 3. Plants planted along public rights of way in Section 3 shall be consistent with those utilized for such purpose in Sections 1 and 2.

I acknowledge that these restrictions and conditions are offered voluntarily and with knowledge of the guidelines stated in the *Filing Addendum*. If additional space is needed, this form may be copied. Each page must be signed by all property owners.

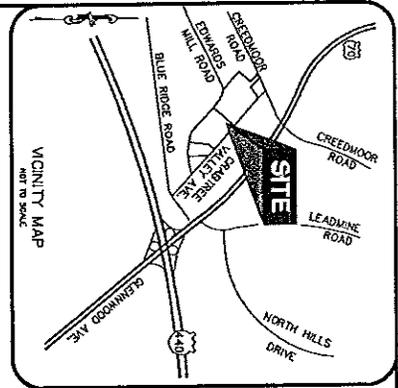
**ALL CONDITIONAL PAGES MUST BE SIGNED BY ALL PROPERTY OWNERS**

Signature(s)  
Weingarten Investments Inc.

By: 

Print Name and Title: William E Coats, Regional Development Director  
Date: August 15, 2011

2-9-11

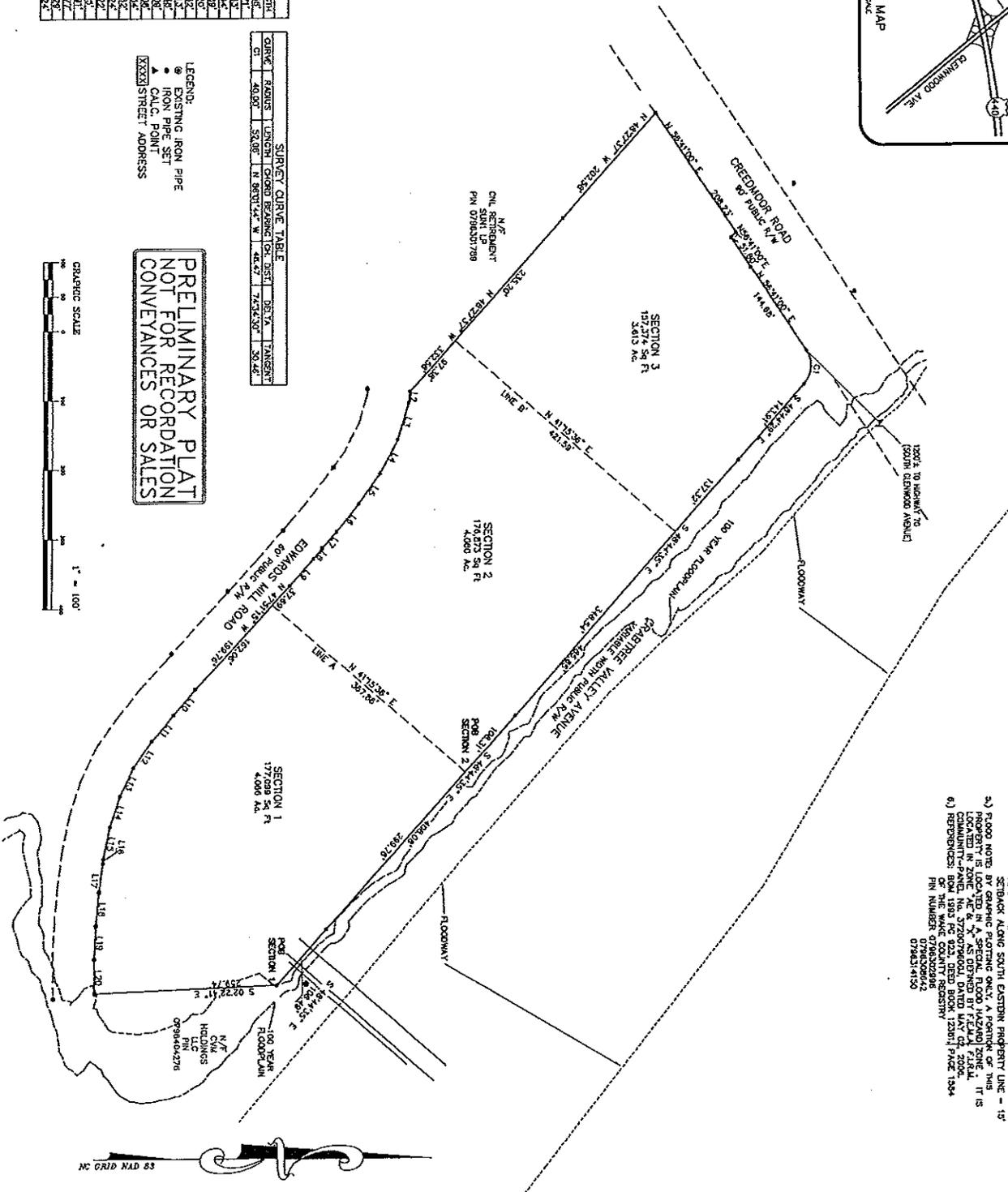


LINE	BEARING	LENGTH
L1	S 48°27'17" E	10.84'
L2	N 84°27'42" W	15.71'
L3	N 72°58'58" W	24.83'
L4	N 67°28'08" W	23.48'
L5	N 51°06'14" W	31.20'
L6	N 50°01'32" W	30.82'
L7	N 48°33'32" W	30.45'
L8	N 48°21'55" W	48.98'
L9	N 48°11'02" W	44.08'
L10	N 47°28'01" W	46.24'
L11	N 47°28'01" W	46.24'
L12	N 47°28'01" W	46.24'
L13	N 47°28'01" W	46.24'
L14	N 47°28'01" W	46.24'
L15	N 47°28'01" W	46.24'
L16	N 47°28'01" W	46.24'
L17	N 47°28'01" W	46.24'
L18	N 47°28'01" W	46.24'
L19	N 47°28'01" W	46.24'
L20	N 47°28'01" W	46.24'

CURVE	RADIUS	LENGTH	CHORD BEARING	CHORD DIST.	DELTA	TANGENT
C1	40.00'	33.00'	N 89°31'24" W	46.47'	75.52°30"	30.46'

LEGEND:  
 ● EXISTING IRON PIPE  
 ● IRON PIPE SET  
 ▲ CALC. POINT  
 XXXXX STREET ADDRESS

**PRELIMINARY PLAT  
 NOT FOR RECORDATION  
 CONVEYANCES OR SALES**



- GENERAL NOTES**
- 1) THIS IS A ZONING CHART.
  - 2) BEARINGS FOR THIS SURVEY ARE BASED ON NC GRID NAD 83
  - 3) ELEVATIONS FOR THIS SURVEY ARE BASED ON NAVD 88
  - 4) BENCH MARK "M11"
  - 5) BENCH MARK "M12"
  - 6) BUILDING SET BACKS IN TABLE
  - 7) SETBACK ALONG CREEDMOOR ROAD - 30'
  - 8) SETBACK ALONG CRABTREE VALLEY AVENUE - 0'
  - 9) SETBACK ALONG SOUTHEASTERN PROPERTY LINE - 15'
  - 10) PROPERTY IS LOCATED IN A SPECIAL FLOOD HAZARD ZONE - IT IS LOCATED IN ZONE AE & X AS DETERMINED BY FLOOD INSURANCE RATE MAP NO. 1302 FOR 922 DEED BOOK 12301 PAGE 1594 OF THE WAKE COUNTY RESURVEY PIN NUMBER 0798314150 (0798314150)



	ZONING EXHIBIT <b>CRABTREE PLACE</b> CITY OF RALEIGH, WAKE COUNTY, NORTH CAROLINA		REVISIONS: _____ _____ _____	THE JOHN R. McADAMS COMPANY, INC. ENGINEERS/PLANNERS/SURVEYORS RESEARCH TRIANGLE PARK, NC P.O. BOX 14005 ZIP 27709-4005 (919) 361-5000
	PROJECT NO. WCT-08G20 DRAWN BY: RTF SCALE: 1" = 100' DATE: 04 MARCH 2011	PREPARED FOR: WEINGARTEN REALTY INVESTORS 9420 FORUM DRIVE SUITE 101 RALEIGH, NORTH CAROLINA		

## PROPERTY DESCRIPTION SECTION 1

Beginning at an iron pipe at the intersection of the southern right of way of Crabtree Valley Avenue and the northern right of way of Edwards Mill Road, being the POINT OF BEGINNING; thence with the Northern right of way or Edwards Mill Road South  $02^{\circ}22'41''$  East, a distance of 259.74 feet to an iron pipe; thence South  $89^{\circ}18'42''$  West, a distance of 49.24 feet to an iron pipe; thence North  $87^{\circ}19'03''$  West, a distance of 48.29 feet to an iron pipe; thence North  $85^{\circ}10'33''$  West, a distance of 48.77 feet to an iron pipe; thence North  $82^{\circ}56'08''$  West, a distance of 41.91 feet to an iron pipe; thence continuing along said line, a distance of 6.15 feet to an iron pipe; thence North  $78^{\circ}53'22''$  West, a distance of 47.22 feet to an iron pipe; thence North  $72^{\circ}28'01''$  West, a distance of 46.24 feet to an iron pipe; thence North  $64^{\circ}45'21''$  West, a distance of 45.62 feet to an iron pipe; thence North  $56^{\circ}07'08''$  West, a distance of 46.34 feet to an iron pipe; thence North  $51^{\circ}07'02''$  West, a distance of 48.08 feet to an iron pipe; thence North  $49^{\circ}21'35''$  West, a distance of 49.09 feet to an iron pipe; thence North  $47^{\circ}51'15''$  West, a distance of 162.06 feet to a point; thence leaving said right of way North  $41^{\circ}15'36''$  East, a distance of 367.85 feet to a point on the southern right of way of Crabtree Valley Avenue; thence with said right of way South  $48^{\circ}44'35''$  East, a distance of 299.76 feet to an iron pipe; thence continuing along said line, a distance of 106.49 feet to an iron pipe, being the POINT OF BEGINNING, and containing 177,099 square feet or 4.066 acres, more or less .

## PROPERTY DESCRIPTION SECTION 2

Beginning at a point on the southern right of way of Crabtree Valley Avenue, said point being the POINT OF BEGINNING; thence leaving said right of way South 41°15'36" West, a distance of 367.85 feet to a point in the northern right of way of Edwards Mill Road; thence with said right of way North 47°51'15" West, a distance of 37.69 feet to an iron pipe; thence North 48°45'55" West, a distance of 50.48 feet to an iron pipe; thence North 50°01'52" West, a distance of 20.13 feet to an iron pipe; thence continuing along said line, a distance of 30.62 feet to an iron pipe; thence North 51°46'14" West, a distance of 51.20 feet to an iron pipe; thence North 54°56'28" West, a distance of 52.89 feet to an iron pipe; thence North 62°49'06" West, a distance of 54.94 feet to an iron pipe; thence North 72°55'55" West, a distance of 55.63 feet to an iron pipe; thence North 84°02'43" West, a distance of 15.71 feet to an iron pipe; thence North 48°27'37" West, a distance of 97.38 feet to an iron pipe; thence leaving said right of way North 41°15'36" East, a distance of 421.59 feet to a point in the southern right of way of Crabtree Valley Avenue; thence with said right of way South 48°44'35" East, a distance of 348.54 feet to an iron pipe; thence continuing along said line, a distance of 108.31 feet to a point, said point being the POINT OF BEGINNING, and containing 176,872 square feet or 4.060 acres, more or less .

Z-9-11

### PROPERTY DESCRIPTION SECTION 3

Beginning at a point the southern right of way of Crabtree Valley Avenue, said point being the POINT OF BEGINNING; thence leaving said right of way South  $41^{\circ}15'36''$  West, a distance of 421.59 feet to a point on the common property line of CNL Retirement Sun1 LP; thence with said line North  $48^{\circ}27'37''$  West, a distance of 235.20 feet to an iron pipe; thence continuing along said line, a distance of 202.58 feet to an iron pipe on the eastern right of way of Creedmoor Road; thence with said right of way North  $56^{\circ}41'00''$  East, a distance of 208.23 feet to an iron pipe; thence South  $48^{\circ}27'17''$  East, a distance of 10.36 feet to an iron pipe; thence North  $56^{\circ}41'00''$  East, a distance of 51.80 feet to an iron pipe; thence continuing along said line, a distance of 144.68 feet to an iron pipe at the intersection of the eastern right of way of Creedmoor Road and the southern right of way of Crabtree Valley Avenue; thence with the right of way of Crabtree Valley Avenue along a curve to the right having a radius of 40.00 feet, an arc length of 52.06 feet and a chord bearing and distance of South  $86^{\circ}01'45''$  East, 48.47 feet to an iron pipe; thence South  $48^{\circ}44'29''$  East, a distance of 143.91 feet to an iron pipe; thence South  $48^{\circ}44'35''$  East, a distance of 137.32 feet to a point, being the POINT OF BEGINNING, and containing 157,377 square feet or 3.613 acres, more or less .

ARTICLE D. USE, RESIDENTIAL DENSITY, SETBACK, HEIGHT REGULATIONS AND SUMMARY SCHEDULES

**Section 10-2071. SCHEDULE OF PERMITTED LAND USES IN ZONING DISTRICTS**

Principal uses and activities for buildings, structures and land permitted by the Zoning Code are listed within the following schedule. All uses listed are principal uses of land, unless accessory uses are specifically identified. See Article H to determine the method of approval, either by the City staff or City Council. In addition, specific uses may require approval by the Board of Adjustment. The symbols on the schedule indicate categories of approval and certain approval procedures which are not contained in Article H:

- GENERAL USE
  - CONDITIONAL USE
  - SPECIAL USE (CITY COUNCIL)
  - SPECIAL USE (BOARD OF ADJUSTMENT)
  - ▨ SITE PLAN APPROVAL (CITY COUNCIL)
  - ▩ SITE PLAN APPROVAL (PLANNING COMMISSION OR CITY COUNCIL)
- proposed use requires preliminary site plan approval from the Planning Commission or City Council depending on the location, size, and level of intensity (10-2132.2). See Article H.

Notes: LAND USE TERMS appearing in *italics* are defined in §10-2002 (e.g. *Forestry, Existing establishment*).

A BLANK SPACE, regardless of shading, indicates that the use is not permitted in that zoning district, except as may be modified by applicable overlay district (TC-19-92, Section 26). In addition to this schedule, regulations and procedures can also apply which restrict use, design, construction and intensity. Additional requirements include overlay zoning districts, conditional use zoning districts, other zoning district regulations, the Comprehensive Plan, or other regulations. If a use encompasses more than one listing, such as group housing and unit ownership (condominium) development, then the symbol for each listing is applicable. Conversions of uses to other permitted uses, unless otherwise provided in this schedule, are allowed in accordance with applicable regulations relating to the new use. A use not listed is prohibited. Any permitted use may be approved as a unit ownership (condominium) development in accordance with §10-2108 and §10-2122.

**SYMBOLS**

- General Use
- Conditional Use
- Special Use (City Council)
- Special Use (Board of Adjustment)
- ▨ Site Plan Approval (City Council)
- ▩ Preliminary Site Plan Approval (Planning Comm. or City Council may be required)

Editor's Note: Ord. No. 2000-734-TC-189, §1, TC-2-00, changed the "Mobile Home" designation to "Manufactured Home."

**ZONING DISTRICTS**

LAND USE	RURAL RESIDENTIAL (R-1)	RESIDENTIAL - 2	RESIDENTIAL - 4	RESIDENTIAL - 6	MANUFACTURED HOME	RESIDENTIAL - 10	RESIDENTIAL - 15	SPECIAL RESIDENTIAL - 20	RESIDENTIAL - 30	CONSERVATION MANAGEMENT	AGRICULTURAL PRODUCTIVE	OFFICE & INSTITUTION - 1	OFFICE & INSTITUTION - 2	SHOPPING CENTER	NEIGHBORHOOD BUSINESS	THOROUGHFARE DISTRICT	INDUSTRIAL - 1	INDUSTRIAL - 2	
Agriculture*	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Accessory structure (barn, well, storage building)	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Agriculture - general	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Agriculture - restricted	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Fish hatchery and fish farm	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Minor tree removal activity	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Research/art	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Temporary event	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Tree disturbing activity except a minor tree removal activity	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Wildlife sanctuary	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○

\*City Council site plan approval shall be required for buildings eight (8) feet in height or higher, which contain any occupiable room, as defined by the North Carolina State Building Code, or for uses located in the downtown area as set forth in §10-2132.2(b)(4).







LAND USE	ZONING DISTRICTS																								
	RR	R-2	R-4	SP R-6	R-6	MH	R-10	R-15	R-20	SP R-30	R-30	GM	AP	RB	OR-L1	OR-L2	OR-L3	BC	SC	NB	Bus Zone	TD	L-1	L-2	
<b>RESIDENTIAL (continued)</b>																									
Manufactured home park																									
Manufactured home subdivision																									
Multifamily dwelling development (outside cluster unit development)																									
Residential development sales office or model home																									
Residence of a caretaker or watchman accessory to a permitted industrial use, rectory, parish house																									
Rest home																									
Rooming house, boarding house, lodging house or tourist home																									
Room rental in dwelling unit (lodger)																									
Single family detached dwelling unit																									
Sorority house																									
Temporary event																									
Townhouse development (outside cluster unit development)																									
Transitional housing																									
Emergency shelter type A																									
Emergency shelter type B																									
Multi-unit supportive housing residence																									
Religious shelter unit																									
Supportive housing residence																									
Unit-ownership (condominium) development (outside cluster unit development)																									
Utility apartment																									
(accessory to single family dwelling unit)																									

Manufactured home park: Ord. No. 2000-724-TC-189, §1, TC-2-00, 2-15-00;  
 Manufactured home subdivision: Ord. No. 2000-724-TC-189, §1, TC-2-00, 2-15-00;  
 Multifamily...: Ord. No. 2008-530-TC-318, §4, TC-19-08, 1-22-09; Residence of  
 caretaker: Ord. No. 1994-108-TC-25, §4, TC-2-04, 6-5-04; Temp. event: Ord.  
 No. 1997-296-TC-68, §2, TC-23-03, 12-7-03; Sororities: Ord. No. 1994-65-TC-  
 570-TC-118, §5, TC-19-08, 1-22-09; Religious shelter: Ord. No. 1994-65-TC-  
 71, §12, TC-23-03, 4-5-04; Ord. No. 1997-348-TC-201, §14, TC-4-02, 11-4-02;  
 Unit-ownership...: Ord. No. 2000-530-TC-318, §6, TC-19-08, 1-22-09

General Use  
 Conditional Use  
 Special Use (City Council)  
 Special Use (Board of Adjustment)  
 Site Plan Approval (City Council) required  
 Preliminary Site Plan Approval (Planning Comm. or City Council) may be required

LAND USE	ZONING DISTRICTS																									
	RR	R-2	R-4	SP R-6	R-6	MH	R-10	R-15	R-20	SP R-30	R-30	CM	AP	RB	O&I-1	O&I-2	O&I-3	BC	SC	NB	Bus Zone	TD	L-1	L-2		
<b>INSTITUTION / CIVIC / SERVICES*</b>																										
Accessory fund raising activity for residential institutions	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Accessory structure (fence, wall, storage building)	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Art gallery	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Governmental	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Non-governmental	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Cemetery	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Not for profit - church, family, government, including mausoleum and columbarium	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Commercial	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Charitable institution	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Church, synagogue or religious education building	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Civic club	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Civic / convention center and assembly hall	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Governmental	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Non-governmental	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Correctional / penal facility	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Governmental	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Non-governmental	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Crematory	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Day care facility (child or adult)	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
50 or less enrollees	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Greater than 50 enrollees	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Family child care home	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Fire station, police precinct, training facility (not including outdoor firing range), and other emergency service facility	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Funeral home	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	
Governmental building and grounds not otherwise listed in this schedule	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	

General Use  
 Conditional Use  
 Special Use (City Council)  
 Special Use (Board of Adjustment)  
 Site Plan Approval (City Council) required  
 Preliminary Site Plan Approval (Planning Comm. or City Council) may be required

(Cemetery: Ord. No. 2007-178-TC-300, §1, TC-19-06, 3-6-07)

\*City Council site plan approval shall be required for buildings eighty (80) feet in height or higher, which contain any occupiable room, as defined by the North Carolina State Building Code, or for uses located in the downtown area as set forth in §10-2132.2(b)(4).

LAND USE	ZONING DISTRICTS
<b>INSTITUTION / CIVIC / SERVICES (cont.)</b> Hospital (medical / psychiatric) Library or museum Governmental Non-governmental Monastery and convent Orphanage Schools Public (elementary, middle and high) Private/parochial (elementary, middle and high) Institution of higher education, including college and university Technical and vocational institution Specialty school (as a limited home business in residential districts and Residential Business District) Special care facility 50 or less enrollees Greater than 50 enrollees Temporary event Veterinary hospital Without a kennel/cattery With a kennel/cattery With the boarding of well animals <small>(Art gallery; Ord. No. 1995-555-TC-100; §2, TC-1-95, 2-7-95, 5-95, 6-95, 7-95, 8-95, 9-95, 10-95, 11-95, 12-95, 1-96, 2-96, 3-96, 4-96, 5-96, 6-96, 7-96, 8-96, 9-96, 10-96, 11-96, 12-96, 1-97, 2-97, 3-97, 4-97, 5-97, 6-97, 7-97, 8-97, 9-97, 10-97, 11-97, 12-97, 1-98, 2-98, 3-98, 4-98, 5-98, 6-98, 7-98, 8-98, 9-98, 10-98, 11-98, 12-98, 1-99, 2-99, 3-99, 4-99, 5-99, 6-99, 7-99, 8-99, 9-99, 10-99, 11-99, 12-99, 1-00, 2-00, 3-00, 4-00, 5-00, 6-00, 7-00, 8-00, 9-00, 10-00, 11-00, 12-00, 1-01, 2-01, 3-01, 4-01, 5-01, 6-01, 7-01, 8-01, 9-01, 10-01, 11-01, 12-01, 1-02, 2-02, 3-02, 4-02, 5-02, 6-02, 7-02, 8-02, 9-02, 10-02, 11-02, 12-02, 1-03, 2-03, 3-03, 4-03, 5-03, 6-03, 7-03, 8-03, 9-03, 10-03, 11-03, 12-03, 1-04, 2-04, 3-04, 4-04, 5-04, 6-04, 7-04, 8-04, 9-04, 10-04, 11-04, 12-04, 1-05, 2-05, 3-05, 4-05, 5-05, 6-05, 7-05, 8-05, 9-05, 10-05, 11-05, 12-05, 1-06, 2-06, 3-06, 4-06, 5-06, 6-06, 7-06, 8-06, 9-06, 10-06, 11-06, 12-06, 1-07, 2-07, 3-07, 4-07, 5-07, 6-07, 7-07, 8-07, 9-07, 10-07, 11-07, 12-07, 1-08, 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LAND USE	ZONING DISTRICTS																							
	RR	R-2	R-4	Sp R-6	MH	R-10	R-15	R-20	Sp R-30	R-30	CM	AP	RB	O&I-1	O&I-2	O&I-3	BC	SC	NB	Bus Zone	TD	L-1	L-2	
<b>OFFICE*</b>	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
<i>Auxiliary structure (fence, wall, storage building)</i>	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Office, agency, or studio of a professional or business agent, or political, labor or service association, including, but not limited to the following:	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Finance - banker, bonding agent, finance agent, insurance adjuster, insurance agent, investment agent and mortgage agent.	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Medical - association of nurses, chiropractor, chiropract, dentist, dental supply company serving dentists and dental laboratories only, nurse, optometrist, osteopath, physician, plasma center, clinic and veterinarian.	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Professional / service - accountant, advertising agent, architect, artist, broker, copy writer, cosmetician (excluding barbering as defined in G.S. 86A-2), engineer, engineering research laboratory, government, hair replacement, landscape architect, lawyer, masseur, office business machine agent, publicity agent, realtor, secretarial agent, mail order store and travel agent.	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Other professional or service office, studio or agency not otherwise listed as permitted in the zoning district	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
<i>Office center</i>	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
School, church or government building adaptively reused as an office, agency or studio, operating on a not for profit basis, for a professional, business, political, or labor or service association or agency	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Temporary event (Ord. No. 1983-286-10-69, §§ 12.7-90)	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
<i>Unit - Ownership (administrative) development</i>	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○

General Use  
 Conditional Use  
 Special Use (City Council)  
 Special Use (Board of Adjustment)  
 Site Plan Approval (City Council) required  
 Preliminary Site Plan Approval (Planning Comm. or City Council) may be required

\*City Council site plan approval shall be required for buildings eighty (80) feet in height or higher, which contain any occupiable room, as defined by the North Carolina State Building Code, or for uses located in the downtown area as set forth in § 10-2-132.2(b)(4).

LAND USE	ZONING DISTRICTS																								
	RR-2	R-2	SP R-4	R-6	MH	R-10	R-15	R-20	SP R-30	R-30	CM	AP	RB	O&L-1	O&L-2	O&L-3	BC	SC	NB	Bus Zone	TJ	L-1	L-2		
<b>COMMERCIAL*</b>																									
Accessory structure (fence, wall storage building)	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Adult establishment																									
Bank																									
With no drive-thru window and with or without an automated teller machine attached to principal building																									
With drive-thru and/or automated teller machine (detached)																									
All other																									
Bar, nightclub, tavern, lounge																									
Beauty, nail and manicure, cosmetic art and barber shop																									
Bed and breakfast inn																									
Carwash facility																									
Located within two hundred (200) feet from the property line of either any lot which is developed with a dwelling, congregate care or congregate living structure or vacant lot located in a residential zoning district.																									
All other																									
Christmas tree, greens and ornamental outdoor display area sales between Nov. 15 and Dec. 31																									
(Bank: Ord. No. 1999-816-TC-181, § 25, TC-3-99, 8-3-99; Bar... Ord. No. 1999-816-TC-181, § 25, TC-3-99, 8-3-99; Beauty, nail and manicure, cosmetic art and barber-shop: Ord. No. 1994-408-TC-75, § 4, TC-9-94, 6-7-94; Carwash facility: Ord. No. 2008-362-TC-308, § 2, TC-9-07, 3-4-08; Ord. No. 2003-490-TC-238, § 2, TC-6-03, 8-5-03; Christmas tree...: Ord. No. 1998-301-TC-164, § 1, TC-4-98, 4-7-98)																									

General Use  
 Conditional Use  
 Special Use (City Council)  
 Special Use (Board of Adjustment)  
 Site Plan Approval (City Council) required  
 Preliminary Site Plan Approval (Planning Comm. or City Council) may be required

\*City Council site plan approval shall be required for buildings eighty (80) feet in height or higher, which contain any occupiable room, as defined by the North Carolina State Building Code, or for uses located in the downtown area as set forth in §10-2132.2(b)(4).

LAND USE	ZONING DISTRICTS																									
	RR	R-2	R-4	SP R-6	R-6	MH	R-10	R-15	R-20	SP R-30	R-30	CM	AP	HB	O-1-1	O-1-2	O-1-3	BC	SC	NB	Bus Zone	TD	L-1	L-2		
<b>COMMERCIAL (continued)</b>																										
Communications																										
<i>Dish antenna</i>																										
Radio and television studio																										
Telecommunication tower located on a building constructed for purposes other than supporting telecommunications equipment, and no taller than 30 ft. or 30% of the height of the building, whichever is greater	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●	●
Telecommunication towers less than 250 ft. in height	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Telecommunication towers less than 250 ft. in height located on a residential institution premises	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Telecommunication tower of 250 ft. in height or greater, or does not meet the conditional use standards of a tower less than 250 ft. in height	□	□	□	□	□	□	□	□	□	□	□	□	□	□	□	□	□	□	□	□	□	□	□	□	□	□
Telegraph office	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Dance, recording, music studio																										
<i>Eating establishment</i>																										
With no drive-thru, drive-in service and with no alcohol sales for on-premise consumption																										
With no drive-thru, drive-in service and with alcohol sales for on-premise consumption																										
With drive-thru, drive-in service, and with or without alcohol sales for on-premise consumption																										
Exterminating service																										
<i>Food store - retail</i>																										
(Communications: Ord. No. 1994-372-TC-72, §1, TC-3-94, 4-19-94; Ord. No. 1995-682-TC-113, §2, TC-15-95, 8-1-95; Ord. No. 2000-732-TC-188, §1, TC-17-99, 2-15-00; Ord. No. 2003-373-TC-231, §9, TC-23-02, 2-4-03; <i>Eating establishment</i> : Ord. No. 1998-616-TC-181, §25, TC-3-99, 8-3-99; Ord. No. 2003-373-TC-231, §10, TC-28-02, 2-4-03; <i>Food Store-retail</i> : Ord. No. 1999-616-TC-181, §25, TC-3-99, 8-3-99)																										

General Use  
 Conditional Use  
 Special Use (City Council)  
 Special Use (Board of Adjustment)  
 Site Plan Approval (City Council) required  
 Preliminary Site Plan Approval (Planning Comm. or City Council) may be required

LAND USE	ZONING DISTRICTS																								
	RR-2	RR-4	SP R-6	R-6	MH	R-10	R-15	R-20	SP R-30	R-30	CM	AP	RB	O-1-1	O-1-2	O-1-3	BC	SC	NB	Bus Zone	TD	L-1	L-2		
<b>COMMERCIAL (continued)</b>																									
<i>Hotel / Motel</i>																									
Without any lodging or dwelling units																									
Containing one (1) or more lodging units without either a microwave oven or one (1) or more cubic feet capacity, a burner cook top, or refrigerator with more than seven (7) cubic feet																									
Containing one (1) or more lodging units with a burner cook top and/or refrigerator with more than seven (7) cubic feet																									
Containing one (1) or more dwelling units with cooking facilities and refrigerators of unlimited size																									
Interim use, redevelopment area																									
Kennel / cattery																									
<i>Mixed use Master Plan Development</i>																									
Movie theater - indoor																									
Movie theater - outdoor																									
Parking facility - principal use																									
<i>Residential institution (off-site)</i>																									
Parking deck/garage																									
Parking lot																									
<i>Motorpool</i>																									
Plant nursery, fruit and vegetable stand																									
Post office																									
(Hotel/motel: Ord. No. 1996-931-TC-138, §3, TC-27-95, 7-2-96; Ord. No. 1997-248-TC-160, §1, TC-19-97, 12-2-97; Mixed use...; Ord. No. 1999-616-TC-181, §25, TC-3-99, 8-3-99; Parking facility - principal use: Ord. No. 1999-616-TC-181, §§25, 27, TC-3-99, 8-3-99)																									

Special Use (City Council)  
 Special Use (Board of Adjustment)  
 Site Plan Approval (City Council) required  
 Preliminary Site Plan Approval (Planning Comm. or City Council) may be required

General Use  
 Conditional Use

LAND USE	ZONING DISTRICTS																									
	RR	R-2	R-4	SP R-6	R-6	MH	R-10	R-15	R-20	SP R-30	R-30	CM	AP	RB	0&L-1	0&L-2	0&L-3	BC	SC	NB	Bus Zone	TD	L-1	L-2		
<b>COMMERCIAL (continued)</b>																										
<i>Retail sales</i>																										
<i>Residential related service (e.g. beauty shop, drug store, eating establishment, gift shop, laundry, newsstand)</i>	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
<i>Nonresidential related service (general) (e.g. eating establishment, flower shop, gift shop, laundry and dry cleaning (pick-up), newsstand, office supply sales)</i>																										
<i>Personal service (laundry and dry cleaning (with pick-up or walk-up services), photography studio, shoe repair shop...)</i>																										
<i>Convenience (apparel, books, drugs, film developing, florist, furniture, gifts, hardware, jewelry, laundries with or without drive-thru, music, shoes, sporting goods, toys...)</i>																										
<i>Pawn shop</i>																										
<i>Outdoor Mobile Vending Cart (see Retail sales-outdoor mobile vending cart)</i>																										
<i>General (e.g. department store, discount store, home building supply store)</i>																										
<i>(Retail sales: Ord. No. 1994-408-TC-75, §4, TC-9-94, 6-7-94; Ord. No. 1999-616-TC-181, §§24, 26, TC-3-99, §-3-99; Convenience: Ord. No. 1999-616-TC-181, §24, TC-3-99, 8-3-99; General: Ord. No. 1999-616-TC-181, §24, TC-3-99, 8-3-99; Outdoor: Ord. No. 2006-985-TC-584, §2, TC-17-05, 4-4-06; Pawn shop: Ord. No. 2009-536-TC-320, §2, TC-17-08, 2-17-09)</i>																										

General Use       Special Use (City Council)       Site Plan Approval (City Council) required  
 Conditional Use       Special Use (Board of Adjustment)       Preliminary Site Plan Approval (Planning Comm. or City Council) may be required

LAND USE	ZONING DISTRICTS
<p><b>COMMERCIAL (continued)</b></p> <p><i>Highway (e.g. equipment sales/repair, vehicle sales/rental, automotive service and repair facility)</i></p> <p><i>Retail sales not otherwise listed as a permitted general or conditional use</i></p> <p><i>(Highway: Ord. No. 1999-616-TC-181, §24, TC-3-99, 8-3-99; Retail sales: Ord. No. 1999-616-TC-181, §24, TC-3-99, 8-3-99)</i></p>	<p>RR</p> <p>R-2</p> <p>R-4</p> <p>SP R-6</p> <p>R-6</p> <p>MH</p> <p>R-10</p> <p>R-15</p> <p>R-20</p> <p>SP R-30</p> <p>R-30</p> <p>CM</p> <p>AP</p> <p>RB</p> <p>O&amp;I-1</p> <p>O&amp;I-2</p> <p>O&amp;I-3</p> <p>BC</p> <p>SC</p> <p>NB</p> <p>Bus Zone</p> <p>TD</p> <p>L-1</p> <p>L-2</p>

General Use  
 Conditional Use  
 Special Use (City Council)  
 Special Use (Board of Adjustment)  
 Size Plan Approval (City Council) required  
 Preliminary Size Plan Approval (Planning Comm. or City Council) may be required



LAND USE	ZONING DISTRICTS																							
	RR	R-2	R-4	Sp R-6	R-6	MH	R-10	R-15	R-20	Sp R-30	CM	AP	RB	O&L-1	O&L-2	O&L-3	BC	SC	NB	Bus Zone	TD	L-1	L-2	
<b>INDUSTRIAL*</b>																								
Accessory structure (fence, wall, storage building)	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○	○
Billboard (off-premise sign)																								
Bottling plant																								
Bulk products (storing, sorting, breaking)																								
Bulk storage of flammable and combustible liquids if a governmental water supply is available for firefighting purposes																								
Incinerator																								
Machine shop																								
<b>Manufacturing</b>																								
Custom (store / shop for custom work)																								
Specialized (assembly of scientific measuring, clocks, medical, musical instruments, watches...)																								
Restricted (within the First Fire District - see map in § 10-2076(a))																								
General																								
Mining and quarrying																								
Mini warehouse storage facility																								
Mulch or compost processing facility																								
Outdoor storage																								
Building materials or contractors construction equipment																								
Coal yard, lumber yard and lumber mill																								
<b>Recyclable material</b>																								
Unlicensed, uninspected, wrecked, crushed, dismantled or partially dismantled vehicles (more than one (1))																								
Reservoir and water control structures																								
Scrap materials - indoor storage																								

(Match: Ord. No. 2009-669-TC-338, §5, TC-8-09, 11-3-09)

General Use  
 Conditional Use  
 Special Use (City Council)  
 Special Use (Board of Adjustment)  
 Site Plan Approval (City Council) required  
 Preliminary Site Plan Approval (Planning Comm. or City Council) may be required

\*City Council site plan approval shall be required for buildings eighty (80) feet in height or higher, which contain any occupiable room, as defined by the North Carolina State Building Code, or for uses located in the downtown area as set forth in §10-2132.2(b)(4).

ZONING DISTRICTS	
LAND USE	INDUSTRIAL (continued)
	<p>Solid waste</p> <p>Indoor reclamation</p> <p>Outdoor reclamation</p> <p>Landfill (debris from on-site)</p> <p>Landfill (all other lawfully approved)</p> <p>Temporary event</p> <p>Transportation</p> <p>Airfield or landing strip</p> <p>Heliport</p> <p>Heliports serving hospitals licensed by the State of North Carolina pursuant to N.C. Gen. Stat. §131E-176 et seq. and that operates acute care beds as defined by 10A N.C.A.C. 14C.3801</p> <p>Heliports serving all other uses</p> <p>Railroad freight station or stop</p> <p>Passenger transit station or stop</p> <p>Passenger transit stop</p> <p>Taxicab dispatch stand</p> <p>Taxicab stand</p> <p>Terminal, facility, railroad roundhouse and depot</p>
RR	
R-2	
R-4	
Sp R-6	
R-6	
MH	
R-10	
R-15	
R-20	
Sp R-30	
R-30	
CM	
AP	
RB	
O&I-1	
O&I-2	
O&I-3	
BC	
SC	
NB	
Bus Zone	
TD	
L-1	
L-2	

General Use  
 Conditional Use  
 Special Use (City Council)  
 Special Use (Board of Adjustment)  
 Site Plan Approval (City Council) required  
 Preliminary Site Plan Approval (Planning Comm. or City Council) may be required

LAND USE	ZONING DISTRICTS																								
	RR	R-2	R-4	SP R-6	R-6	MH	R-10	R-15	R-20	SP R-30	R-30	CM	AP	RB	O&L-1	O&L-2	O&L-3	BC	SC	NB	Bus Zone	TD	F-1	F-2	
<b>INDUSTRIAL (continued)</b>																									
Utilities																									
Conversion of a utility to another utility use or a municipal use																									
Gas plant																									
Power plant																									
Electrical generating plant powered by wind, water or solar																									
Other power plants																									
Substation																									
Utility services																									
Water or sanitary sewer treatment plant																									
Governmental																									
Community																									
Warehousing / distribution center																									
Wholesale laundry, dyeing and dry cleaning																									
Wholesaling																									
<b>NONGOVERNMENTAL USE OR TRADE NOT OTHERWISE PROHIBITED</b>																									
(Outdoor storage...dismantled vehicles; Ord. No. 1993-134-TC-47, §3, TC-1-93, 2-2-93; Railroad freight station or stop; Ord. No. 2004-596-TC-245, §5, TC-14-03, 4-6-04; Recyclable material; Ord. No. 1992-87-TC-400, §4, 11-4-92; Temporary event; Ord. No. 1993-296-TC-68, §2, TC-23-93, 12-7-93; Transportation; Ord. No. 2008-405-TC-312, §86, 7, TC-3-08, 6-3-08; Utilities; Ord. No. 278-TC-326, §3, TC-18-88, 11-20-89)																									

**Editor's note:** Section 45 of Ord. No. 1997-137-TC-153, TC-18-96, adopted June 17, 1997, made major changes to this Schedule of Permitted Land Uses in Zoning Districts. For specific information on these changes, too numerous to history note individually within the schedule, please see Ord. No. 1997-137-TC-153.

General Use       Special Use (City Council)       Site Plan Approval (City Council) required  
 Conditional Use       Special Use (Board of Adjustment)       Preliminary Site Plan Approval (Planning Comm. or City Council) may be required

# EXHIBIT D. Petitioner's Statement on Behalf of Zoning Change

Please use this form only – form may be photocopied. Please type or print. See instructions in *Filing Addendum*

This section is reserved for the applicant to state factual information in support of the rezoning request.

### **Required items of discussion:**

The Planning Department is instructed not to accept any application for amending the official zoning map without a statement prepared by the applicant analyzing the reasonableness of the rezoning request. This statement *shall* address the consistency of the proposed rezoning with the Comprehensive Plan and any other applicable *City*-adopted plan(s), the compatibility of the proposed rezoning with the *property* and surrounding area, and the benefits and detriments of the proposed rezoning for the landowner, the immediate neighbors and the surrounding community.

### **Recommended items of discussion (where applicable):**

1. An error by the City Council in establishing the current zoning classification of the property.
2. How circumstances (land use and future development plans) have so changed since the property was last zoned that its current zoning classification could not properly be applied to it now were it being zoned for the first time.
3. The public need for additional land to be zoned to the classification requested.
4. The impact on public services, facilities, infrastructure, fire and safety, parks and recreation, topography, access to light and air, etc.

### **PETITIONER'S STATEMENT:**

#### **I. Consistency of the proposed map amendment with the Comprehensive Plan ([www.raleighnc.gov](http://www.raleighnc.gov)).**

##### **A. Please state the recommended land use(s) for this property as shown on the Future Land Use Map and discuss the consistency of the proposed land uses:**

The Property is designated Regional Mixed Use on the Future Land Use Map. Regional Mixed Use areas may include high-density housing, office development, hotels, and region-serving retail uses. The proposed land uses permitted by the Shopping Center Conditional Use District and the Shopping Center Conditional Use District with Pedestrian Business Overlay District are consistent with the Future Land Use Map classification for this property.

##### **B. Please state whether the subject property is located within any Area Plan or other City Council-adopted plans and policies and discuss the policies applicable to future development within the plan(s) area.**

The Property is located within the Crabtree Area Plan. The Crabtree Area Plan's goal is for the area to develop as a mixed-use environment with people living, working, and shopping within a walkable urban community. The Property is identified in the Crabtree Area Plan as "Kidds Hill Plaza." Kidds Hill Plaza is identified in the plan as one of the locations where additional retail should be located. The following policies are applicable to the Property: Policy AP-C1 "Crabtree Parking Structures," Policy AP-C2 "Crabtree Area Hillside," Policy AP-C3 "Crabtree Creek," Policy AP-C4 "Crabtree Mall Connections," Policy AP-C5 "Design Unity in the Crabtree Area," and Policy AP-C6 "Crabtree Area Pedestrian Circulation Plan." The proposed map amendment is consistent with all applicable Crabtree Area Plan policies.

# EXHIBIT D. Petitioner's Statement on Behalf of Zoning Change

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**C. Is the proposed map amendment consistent or inconsistent with the Comprehensive Plan and other City Council-adopted plans and policies? All references to Comprehensive Plan policies should include both the policy number (e.g. LU 4.5) and short title (e.g. "Connectivity").**

The proposed map amendment is consistent with the Comprehensive Plan and the Crabtree Area Plan. Specifically, as noted above, the proposed map amendment is consistent with Policy LU 1.2 "Future Land Use Map and Zoning Consistency." Because the proposed map amendment is consistent with Comprehensive Plan, the conditions of the proposed map amendment are consistent with Policy LU 1.3 "Conditional Use District Consistency." The proposed map amendment would permit the Property to be developed for high-density residential and mixed-use development, which is consistent with Policy LU 2.2 "Compact Development." The proposed map amendment would facilitate the redevelopment of the former Kidds Hill Plaza, which is consistent with Policy LU 7.2 "Shopping Center Reuse" and Policy LU 8.10 "Infill Development." A significant portion of the proposed map amendment would be part of a Pedestrial Business Overlay District, which is consistent with Policy LU 7.6 "Pedestrian Friendly Community Uses." Based on the foregoing, the proposed map amendment is consistent with the Comprehensive Plan.

**II. Compatibility of the proposed map amendment with the property and the surrounding area.**

**A. Description of land uses within the surrounding area (residential housing types, parks, institutional uses, commercial uses, large parking lots, thoroughfares and collector streets, transit facilities):**

Land to the east of the Property is developed as Crabtree Valley Mall. Land to the north of the Property is developed for use as offices. Land to the west of the Property is developed for use a senior living community and for residential uses. Land to the south of the Property is developed as apartments. The Property is bounded on the north by Creedmoor Road, a secondary arterial. Currently, the Property is bounded on the west by Crabtree Valley Avenue. The property is partially bounded on the east by Edwards Mill Road.

**B. Description of existing Zoning patterns (zoning districts including overlay districts) and existing built environment (densities, building heights, setbacks, tree cover, buffer yards):**

Land to the east of the Property is zoned Shopping Center District. Land to the north of the Property is zoned Office & Institution-2. Land to the west of the Property is zoned Office & Institution-1 District and Residential-6. Land to the south of the Property is zoned Shopping Center District and Shopping Center Conditional Use District.

**C. Explanation of how the proposed zoning map amendment is compatible with the suitability of the property for particular uses and the character of the surrounding area:**

The proposed zoning map amendment maintains the mix of Residential, Office, and Commercial land uses authorized for the Property, but at densities less than those authorized in the existing zoning. Therefore, this proposed zoning map amendment is compatible with the character of the surrounding area because it facilitates development of the property for uses already envisioned by the existing zoning. Also, because the Property is immediately adjacent to Crabtree Valley Mall and is located in a Regional Mixed Use Center, it is an ideal location for development that includes a mix of uses. Based on the

# EXHIBIT D. Petitioner's Statement on Behalf of Zoning Change

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forgoing, the proposed map amendment is compatible with the suitability of the property for particular uses and the character of the surrounding area.

### III. Benefits and detriments of the proposed map amendment.

#### A. For the landowner(s):

The proposed map amendment benefits the landowner by facilitating development of the Property.

#### B. For the immediate neighbors:

The proposed map amendment benefits the immediate neighbors by facilitating development of the Property, which is currently vacant, for a mix of uses at densities less than those authorized by current zoning. There are no known detriments of the proposed map amendment for the immediate neighbors.

#### C. For the surrounding community:

The proposed map amendment benefits the surrounding community by facilitating development of the Property, which is currently vacant, for a mix of uses at densities less than those authorized by current zoning and in a manner consistent with the Comprehensive Plan. There are no known detriments of the proposed map amendment for the surrounding community.

### IV. Does the rezoning of this property provide a significant benefit which is not available to the surrounding properties? Explain:

The rezoning of the Property does not provide a significant benefit which is not available to the surrounding properties. Almost all of the land surrounding the Property is zoned Shopping Center District, Shopping Center Conditional Use District, Office & Industrial-1 or Office & Industrial-2. All of those zoning districts permit many of the uses permitted by the proposed map amendment.

#### Explain why the characteristics of the subject property support the proposed map amendment as reasonable and in the public interest.

The proposed map amendment does not materially change what can be developed on the property. It reduces the density at which those uses can be developed and eliminates the PDD Master Plan currently applicable to the Property, which has been found to include elements that make it unfeasible. The proposed map amendment would place more realistic conditions on the Property, and make development of the Property feasible without changing the character of the land uses that can be developed on the Property or increasing the overall intensity of development. Given the location of the Property immediately adjacent to Crabtree Valley Mall and in a Regional Mixed Use Center, and the similar uses authorized the existing zoning and the proposed zoning, the characteristics of the Property support the proposed map amendment as reasonable and in the public interest.

# EXHIBIT D. Petitioner's Statement on Behalf of Zoning Change

Please use this form only – form may be photocopied. Please type or print. See instructions in *Filing Addendum*

## V. Recommended items of discussion (where applicable).

- a. **An error by the City Council in establishing the current zoning classification of the property.**

Not applicable.

- b. **How circumstances (land use and future development plans) have so changed since the property was last zoned that its current zoning classification could not properly be applied to it now were it being zoned for the first time.**

Not applicable.

- c. **The public need for additional land to be zoned to the classification requested.**

Not applicable.

- d. **The impact on public services, facilities, infrastructure, fire and safety, parks and recreation, topography, access to light and air, etc.**

Not applicable.

- e. **How the rezoning advances the fundamental purposes of zoning as set forth in the N.C. enabling legislation.**

The rezoning advances the fundamental purposes of zoning by protecting and promoting the public welfare and safety and regulating land use in accordance with a comprehensive plan. The rezoning permits development in accordance with the City's adopted Comprehensive Plan, including the Crabtree Area Plan and the Future Land Use Plan Map.

## VI. Other arguments on behalf of the map amendment requested.

There are no other arguments on behalf of the proposed map amendment at this time.



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## MEMORANDUM

Date: June 2, 2011

To: Dhanya Sandeep  
City of Raleigh, Planning Department

From: Bob Zumwalt, RLA  
Director -Land Planning

Re: **Crabtree Place – Streetscape and Parking Plan Case #Z-9-11  
Design Guidelines for Mixed-Use Developments  
MFI-11000**

### APPENDIX B: Table UD-1 Design Guidelines for Mixed-Use Developments

#### Elements of Mixed-Use Areas

1. *All Mixed-Use Areas should generally provide retail (such as eating establishments, food stores, and banks), office, and residential uses within walking distance of each other.*

**Response:** *The overall project will ultimately provide a mixture of uses, possibly including multi-family, retail and the potential for office uses and/or hospitality.*

#### Mixed-Use Areas /Transition to Surrounding Neighborhoods

2. *Within all Mixed-Use Areas buildings that are adjacent to lower density neighborhoods should transition (height, design, distance and/or landscaping) to the lower heights or be comparable in height and massing.*

**Response:** *The property is bound on the north and east by Crabtree Valley Mall property. To the west is Creedmoor Road and to the southwest is a multi-story assisted care facility on a parcel zoned O & I-1. South across Edwards Mill Road is an area zoned R-6 largely developed for duplex residences. The relationship of this area to the proposed development is such that the finished floor elevation of the area ranges from 15'-30' above the projected finished floor of the of the development proposed in Section 1 of the property. Due to this dramatic grade change, it is likely that only the 3<sup>rd</sup> and 4<sup>th</sup> floors of the building will be visible from Edwards Mill Road. Thus the height of the building will be in keeping with the one and two story residences across the street.*

#### Mixed-Use Areas /The Block, The Street and The Corridor

3. *A mixed use area's road network should connect directly into the neighborhood road network of the surrounding community, providing*

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*multiple paths for movement to and through the mixed use area. In this way, trips made from the surrounding residential neighborhood(s) to the mixed use area should be possible without requiring travel along a major thoroughfare or arterial.*

***Response: The entire infrastructure surrounding the property is currently in place to serve this development. At the time of site plan approval, pedestrian connections and/or corridors will be provided to the surrounding streets, allowing for the free flow of pedestrians to and from the site, as adjacent grades permit.***

- 4. Streets should interconnect within a development and with adjoining development. Cul-de-sacs or dead-end streets are generally discouraged except where topographic conditions and/or exterior lot line configurations offer no practical alternatives for connection or through traffic. Street stubs should be provided with development adjacent to open land to provide for future connections. Streets should be planned with due regard to the designated corridors shown on the Thoroughfare Plan.*

***Response: The entire infrastructure surrounding the property is currently in place to serve this development. At the time of site plan, additional detail will be provided in the form of vehicular and pedestrian connections to the site and adjoining uses.***

- 5. Block faces should have a length generally not exceeding 660 feet.*  
***Response: Specific block configuration will be determined at the time of site plan. In no case will a block length exceed 660 feet without providing either a private street or pedestrian corridor.***

### **Site Design/Building Placement**

- 6. A primary task of all urban architecture and landscape design is the physical definition of streets and public spaces as places of shared use. Streets should be lined by buildings rather than parking lots and should provide interest especially for pedestrians. Garage entrances and/or loading areas should be located at the side or rear of a property.*

***Response: Along the Edwards Mill Road frontage of the property, specifically Section 1 of the property, we are proposing to line the frontage with the proposed structure and incorporate a wide pedestrian walkway along with enhanced plantings and seating areas. We are proposing a minimum building setback along this frontage of 0' with no off-street parking proposed between the building and the street.***



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*Due to the intense vehicular orientation of Creedmoor Road along with the significant cross slope of the site along this frontage, it is not practical to view this streetscape as a viable place for building facades and pedestrian walkways to interact in a functional manner. It is our intent to create internally focused streetscapes along private drives or pedestrian corridors while inviting pedestrians to interact with the space and the proposed uses in a less vehicular oriented relationship. This level of detail will be provided at the time of site plan approval.*

7. Buildings should be located close to the pedestrian street (within 25 feet of the curb), with off-street parking behind and/or beside the buildings.

*Response: At the present time, specific land uses are planned only for Section 1 of the area proposed for rezoning. Within Section 1, the building will be located within 0'-25' of the curb line of the future relocated Crabtree Valley Avenue (aka, Edwards Mill Road). In addition, off-street parking will not be located between the street and the building but rather within internal parking structures. The west portion of the property fronting on Creedmoor Road will limit off-street parking between the building and the street to one double bay of parking. All other parking will be provided internal to the project or within parking structures. This level of detail will be provided at the time of site plan approval.*

8. Block faces should have a length generally not exceeding 660 feet.

*Response: We will be in compliance with this requirement. This level of detail will be shown at the time of site plan approval.*

## Site Design/Urban Open Space

9. To ensure that urban open space is well-used, it is essential to locate and design it carefully. The space should be located where it is visible and easily accessible from public areas (building entrances, sidewalks). Take views and sun exposure into account as well.

*Response: We intend to provide a variety of open space opportunities in the form of linear parks along the frontages that will be open to the public, as well as some internal private gardens for Section 1 of the property to be used by the residents of the development. This level of detail will be shown at the time of site plan approval.*

10. New urban spaces should contain direct access from the adjacent streets. They should be open along the adjacent sidewalks and allow



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for multiple points of entry. They should also be visually permeable from the sidewalk, allowing passersby to see directly into the space.

**Response:** *See pages 5-10 of the Streetscape and Parking Plan for conceptual images of our perimeter urban spaces for Sections 1 and 2 of the area proposed for rezoning. Additional detail will be shown at the time of site plan approval.*

11. The perimeter of urban open spaces should consist of active uses that provide pedestrian traffic for the space including retail, cafés, and restaurants and higher-density residential.

**Response:** *Once the three Sections of the property (as identified in the Zoning Exhibit) are site planned and defined, the relationship between the components can be refined to include these types of active spaces. This level of detail will be defined at the time of site plan approval.*

12. A properly defined urban open space is visually enclosed by the fronting of buildings to create an outdoor "room" that is comfortable to users.

**Response:** *Once the three Sections of the property (as identified in the Zoning Exhibit) are site planned and defined, the relationship between the components can be refined to include these types of active spaces. This level of detail will be defined at the time of site plan approval.*

## Site Design/Public Seating

13. New public spaces should provide seating opportunities.

**Response:** *As identified in the Streetscape and Parking Plan we are proposing multiple seating opportunities along the south and east frontages of the project for Sections 1 and 2 of the property. Additional details will be provided at the time of site plan approval.*

## Site Design/Automobile Parking and Parking Structures

14. Parking lots should not dominate the frontage of pedestrian-oriented streets, interrupt pedestrian routes, or negatively impact surrounding developments.

15. Parking lots should be located behind or in the interior of a block whenever possible. Parking lots should not occupy more than 1/3 of the frontage of the adjacent building or not more than 64 feet, whichever is less



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16. Parking structures are clearly an important and necessary element of the overall urban infrastructure but, given their utilitarian elements, can give serious negative visual effects. New structures should merit the same level of materials and finishes as that a principal building would, care in the use of basic design elements can make a significant improvement.

***Response to 14-16: Within Section 1 of the area proposed for rezoning, no off-street parking will be located between the street and the building but rather within internal parking structures. The west portion of the property fronting on Creedmoor Road will limit off-street parking between the building and the street to one double bay of parking. All other parking will be located internal to the project or within parking structures. This level of detail will be illustrated at the time of site plan approval.***

### **Site Design/Transit Stops**

17. Higher building densities and more intensive land uses should be within walking distance of transit stops, permitting public transit to become a viable alternative to the automobile.
18. Convenient, comfortable pedestrian access between the transit stop and the building entrance should be planned as part of the overall pedestrian network.

***Response to 17-18: The site is located immediately adjacent to transit on both Edwards Mill and Creedmoor Road. In addition, there is a bus transfer station located within walking distance of the site within the parking structure of Crabtree Valley Mall.***

### **Site Design/Environmental Protection**

19. All development should respect natural resources as an essential component of the human environment. The most sensitive landscape areas, both environmentally and visually, are steep slopes greater than 15 percent, watercourses, and floodplains. Any development in these areas should minimize intervention and maintain the natural condition except under extreme circumstances. Where practical, these features should be conserved as open space amenities and incorporated in the overall site design.

***Response: This property will be the redevelopment of an old shopping center. As such, there are no sensitive areas on the property. The limits of development will be defined at the time of site plan.***



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### **Street Design/General Street Design Principles**

20. It is the intent of these guidelines to build streets that are integral components of community design. Streets should be designed as the main public spaces of the City and should be scaled for pedestrians.

21. Sidewalks should be 5-8 feet wide in residential areas and located on both sides of the street. Sidewalks in commercial areas and Pedestrian Business Overlays should be a minimum of 14-18 feet wide to accommodate sidewalk uses such as vendors, merchandising and outdoor seating.

*Response to 20-21: It is our intent to comply with these guidelines as illustrated in our Streetscape and Parking plan. Final details will be shown at the time of site plan approval.*

22. Streets should be designed with street trees planted in a manner appropriate to their function. Commercial streets should have trees which compliment the face of the buildings and which shade the sidewalk. Residential streets should provide for an appropriate canopy, which shadows both the street and sidewalk, and serves as a visual buffer between the street and the home. The typical width of the street landscape strip is 6-8 feet. This width ensures healthy street trees, precludes tree roots from breaking the sidewalk, and provides adequate pedestrian buffering. Street trees should be at least 6 1/4" caliper and should be consistent with the City's landscaping, lighting and street sight distance requirements.

*Response: A detailed landscape plan will be provided at the time of site plan. Street trees will be installed at a minimum of 3" caliper in order to assure their survival and give them the best chance at adapting to the urban environment.*

### **Street Design/Spatial Definition**

23. Buildings should define the streets spatially. Proper spatial definition should be achieved with buildings or other architectural elements (including certain tree plantings) that make up the street edges aligned in a disciplined manner with an appropriate ratio of height to width.

*Response: This level of detail will be illustrated at the time of site plan.*

### **Building Design/Facade Treatment**

24. The primary entrance should be both architecturally and functionally on the front facade of any building facing the primary public street. Such entrances shall be designed to convey their prominence on the fronting facade.



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25. The ground level of the building should offer pedestrian interest along sidewalks. This includes windows entrances, and architectural details. Signage, awnings, and ornamentation are encouraged.

*Response to 24-25: This level of detail will be illustrated at the time of site plan.*

**Building Design/Street Level Activity**

26. The sidewalks should be the principal place of pedestrian movement and casual social interaction. Designs and uses should be complementary to that function.

*Response: This level of detail will be illustrated at the time of site plan.*

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# CRABTREE PLACE

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## Streetscape and Parking Plan

Raleigh, North Carolina  
Case Number Z-09-11

**Date: June 3, 2011**  
*Revised: June 13, 2011*  
*August 2, 2011*

**Owner:**

**WEINGARTEN/INVESTMENTS, INC.**  
9420 Forum Drive, Suite 101  
Raleigh, NC 27615

**Prepared By:**



THE JOHN R. McADAMS  
COMPANY, INC.

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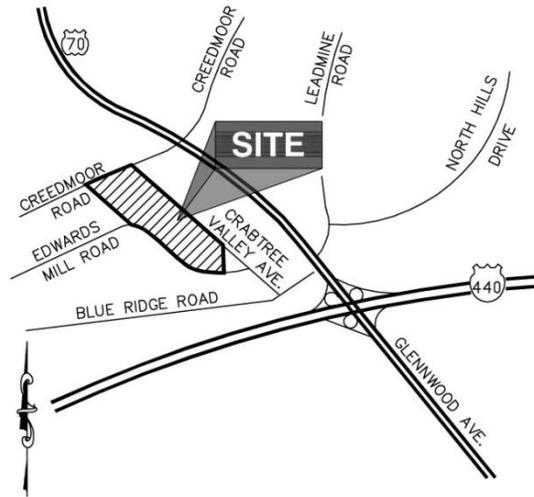
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## Introduction

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### Location Map



### Site History

This 11.73 acre site, once containing a traditional shopping center and a couple of standalone restaurants, was demolished in 2004 in association with plans for re-development. This site's prominent location, adjacent to Crabtree Valley Mall and at the crossroads of two major thoroughfares, makes it a prime candidate for redevelopment into a vibrant center containing a mixture of uses and providing a place for people to live, shop and recreate. The property is currently subject to a Planned Development District that was instigated by a previous owner. As time has passed and the market has changed, the constraints of the current PDD have proven impractical to abide by and thus the current owner is seeking alternate zoning through the implementation of a Shopping Center CUD with a PBOD to facilitate a mixture of complimentary uses on the site.

### Planning Objective

Crabtree Place will incorporate a high density, urban infill residential development located immediately adjacent to Crabtree Valley Mall which is one of the largest retail malls in North Carolina. The incorporation of the PBOD at this location will complete a key link to Crabtree Valley Mall which will help draw pedestrian traffic from the residential at Crabtree Place as well as the existing neighboring communities to the south.

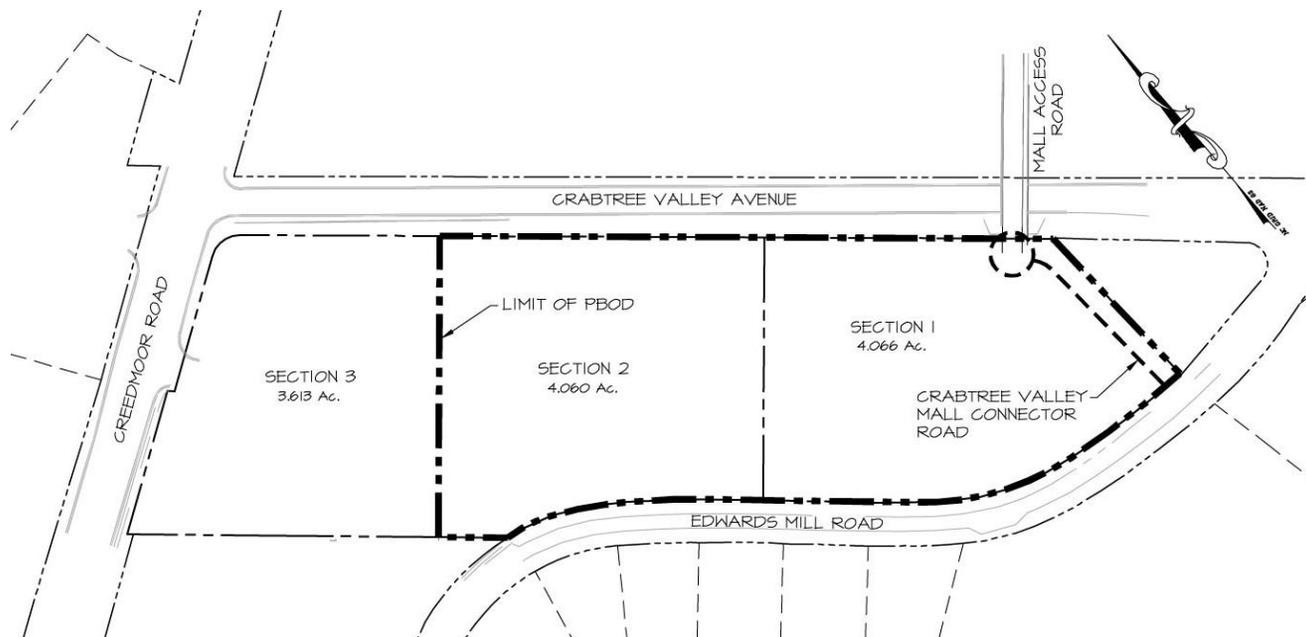
Through the use of site features such as expanded walkways, decorative crosswalks, pedestrian scale lighting and enhanced street furnishings, the project will provide an inviting corridor for pedestrian travel both to and through the project.

The goal of this streetscape and parking plan is to provide a framework for development of sections 1 and 2 of the project as shown on the rezoning exhibit below. The site is currently a missing piece in the framework of this vibrant retail node. By providing these guidelines for development, we can insure that this void is ultimately filled with a pedestrian friendly urban project, unified by a common streetscape theme.

The streetscape and public spaces are intended to support the urban nature this site offers. In order to facilitate an inviting pedestrian scale streetscape, the plan will regulate items such as building setbacks, pedestrian walkways, paving patterns and finishes, landscaping and street furnishings.

By providing this framework, the project will have the opportunity to develop over time and yet still maintain a cohesive development pattern that holds together as a unified project that will stand the test of time.

### Zoning/PBOD Boundary Exhibit



## **Transportation**

The site is located in the southeast quadrant of Creedmoor Road and Crabtree Valley Avenue. In addition, the site is immediately adjacent to Crabtree Valley Mall at a major crossroads of activity for vehicular and bus traffic.

One of the potential challenges this site faces is the unique topography along both the north and south frontages of the site. Crabtree Valley Avenue is currently 20'-25' below the elevation of the site, while Edwards Mill Road to the south rises to 25' above the site. This configuration makes access to the site very difficult. In addition, the City of Raleigh has plans to re-align Crabtree Valley Avenue to intersect with Creedmoor Road on the south side of the property. Under this scenario, the current location of Crabtree Valley Avenue would be vacated. We support the concept of these plans and are working with the City to help define the best approach to accommodating the future conditions along our project's southern frontage. It is unclear at this point as to the timing of these improvements, but for the sake of designing our streetscape, we have assumed that Crabtree Valley Avenue will be relocated. In the event the road is not relocated, we will propose alternate streetscape concepts as discussed below on pages 6-7.

In addition to vehicular transportation, there are bus lines currently running along all frontages of the property, with a transfer station located within walking distance of the site inside the parking structure of Crabtree Valley Mall. Upon approval of a site plan or plans for the development of all or a portion of the 11.73 acres subject to this zoning case, if requested by the City's Transit Division, the owner shall dedicate a transit easement upon the respective site or sites of a size, nature and location acceptable to such Division and provide any improvements, such as a bench and/or shelter, specified by such Division that will be in accordance with its standard policies. The owner shall dedicate up to three (3) transit easements upon the 11.73 acres subject to this zoning case.

This framework of transportation modes offer excellent support for a high density residential project mixed with a number of other uses such as retail, office and/or hospitality.

## **Streetscape Concept**

The overall streetscape concept has been designed to provide a pedestrian scale experience. It is very likely that both of our frontages will ultimately carry a great deal of vehicular traffic, especially after the relocation of Crabtree Valley Avenue. As such, it is important to incorporate streetscape elements that give the pedestrian a sense of safety as they pass through the property.

### ***Re-Aligned Crabtree Valley Avenue Frontage***

In order to protect the pedestrian from vehicular traffic, we plan to intersperse a series of curbside planting beds between the sidewalk and curb, allowing for a row of low plantings as well as seasonal color. In addition, street trees located in tree grates will be provided at a minimum

of 40' o.c. with decorative pedestrian scale lighting, benches, trash receptacles and bike racks placed regularly along the streetscape. The pedestrian walkways will also incorporate brick banding to further accentuate the edges of the walkway and tree grates. We have also incorporated a planting strip between the back of the sidewalk and the building in order to break up the façade of the building and soften the streetscape with additional plantings. Final location of the brick bands and street furnishing will be finalized at site plan approval.

### ***Crabtree Valley Mall Connector Road Frontage***

This connector road will ultimately act as a major access point to Crabtree Valley Mall. As such, we have designed this frontage to slow pass-through traffic by providing on-street parallel parking on both sides of the street. It is likely that this frontage will contain access points to the building so we would anticipate the facade location closer to the back of curb than along the Edwards Mill Frontage. This streetscape contains a mixture of curbside and building-side planters as well as sections where the entire streetscape is paved to facilitate pedestrian circulation. This frontage will also incorporate street trees at a minimum of 40' o.c. with decorative pedestrian scale lighting, benches, trash receptacles and bike racks placed regularly along the streetscape. The actual location of the "Crabtree Valley Mall Connector Road" shown on pages 4 and 16 will be established at the time of site plan approval.

### **Alternative Streetscape Concept (if Crabtree Valley Avenue is not Re-Aligned)**

#### ***Existing Edwards Mill Road***

In the event the re-alignment of Crabtree Valley Avenue is not approved, or, if approved, is unfunded and delayed and existing Edwards Mill Road stays in place, in view of the drastic grade separation between Edwards Mill Road and the finished grade of the project, an alternate streetscape concept will be implemented that locates the 14' wide pedestrian walk adjacent to the building(s) and at the same finished elevation.

#### ***Existing Crabtree Valley Avenue***

If Crabtree Valley Avenue is re-aligned as proposed, or if such re-alignment is approved and funded, there will be no improvements to the streetscape along the existing right of way of Crabtree Valley Avenue. If such re-alignment is approved and is unfunded, and development of the PBOD site commences, streetscape improvements along the right-of-way of existing Crabtree Valley Avenue will be delayed for a period of two years following issuance of a final certificate of occupancy for the project. Upon the expiration of that period, if the re-alignment has been funded, there will be no improvements to the streetscape along such right of way. If funding for the approved re-alignment has not been provided at that time, and if the Planning Director so directs, a walkway 14' in width (consistent with that proposed for re-aligned Crabtree

Valley Avenue) will be constructed along such frontage of the site adjacent to, and at the same grade as, building(s) on the site. If the re-alignment of Crabtree Valley Avenue is not approved, upon development of the PBOD site, the 14' wide walkway adjacent to, and at the grade of, the building(s) will be constructed. If under any circumstance the developer is required to provide a sidewalk at the grade of existing Crabtree Valley Avenue and at the grade of the project, the cumulative width of sidewalks along such frontage of existing Crabtree Valley Avenue will not exceed 14'.

## **Streetscape Standards**

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### **Width of Pedestrian Ways**

There are currently no existing sidewalks adjacent to the boundary of the proposed PBOD. As required, we will provide new sidewalks in accordance with the foregoing text and, as applicable, the sections and plans illustrated below.

**Re-Aligned Crabtree Valley Avenue Frontage** – This frontage will be improved with a full 14' wide streetscape in accordance with City of Raleigh PBOD standards (see illustration below).

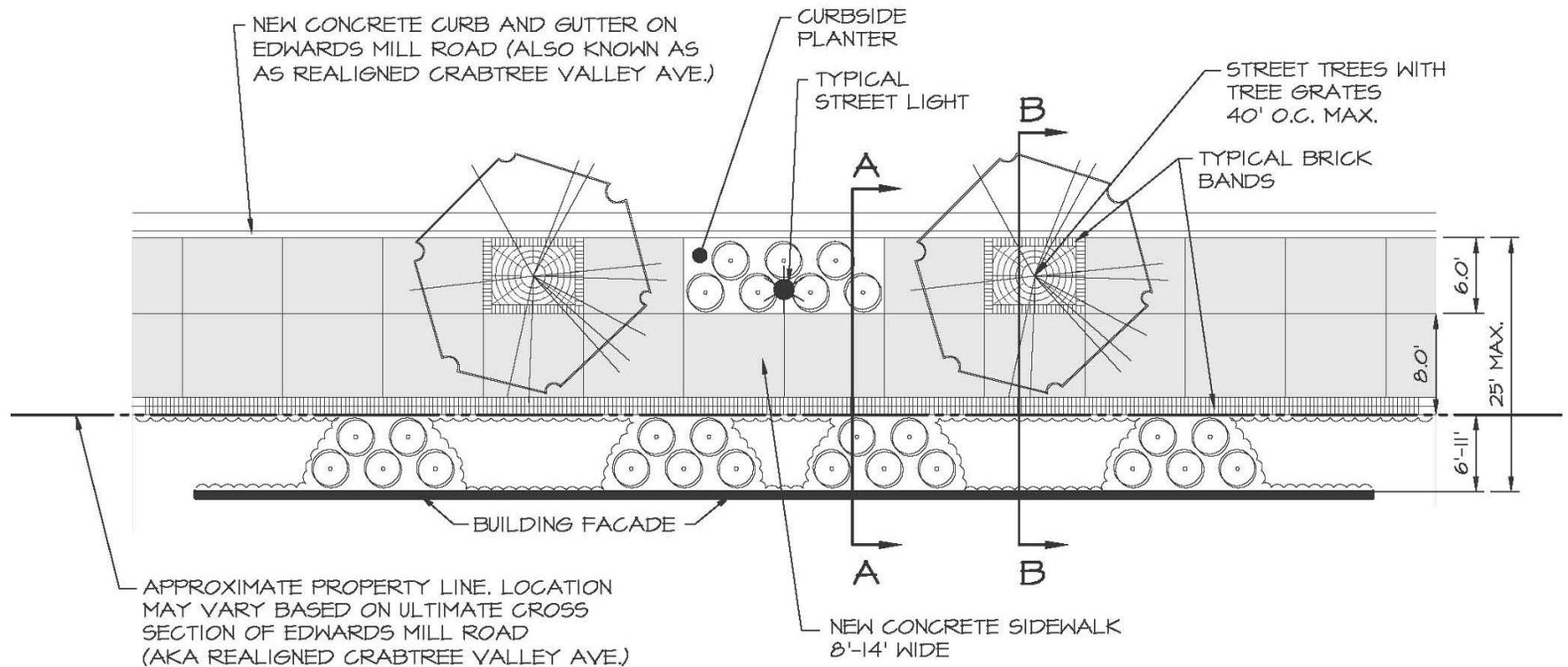
**Crabtree Valley Mall Connector Road** (east frontage) – The connector road between Edwards Mill Road and Crabtree Valley Mall will be improved with a full 14' wide streetscape in accordance with City of Raleigh PBOD standards (see illustration below).

### **Pedestrian Crosswalk locations**

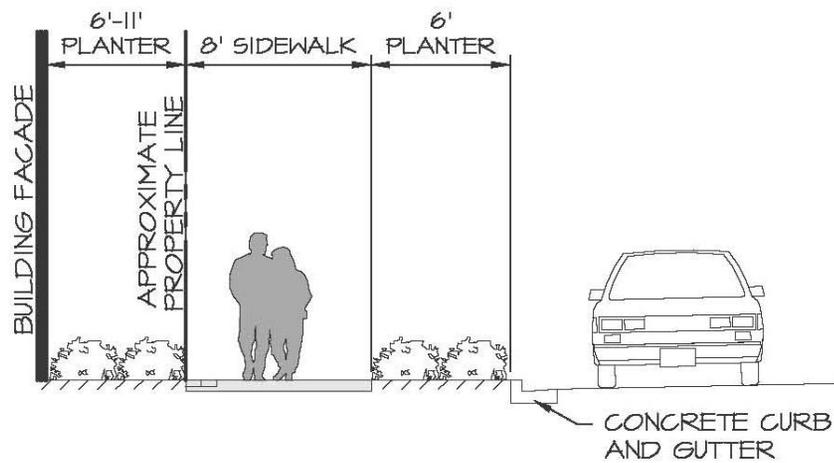
Pedestrian crosswalks will be provided at the intersection of Edwards Mill and Crabtree valley Mall Connector Roads as well as at any point where a street or driveway crosses the pedestrian travel way. Crosswalks will be designed in accordance with accessibility requirements.

## Re-Aligned Crabtree Valley Avenue Plan

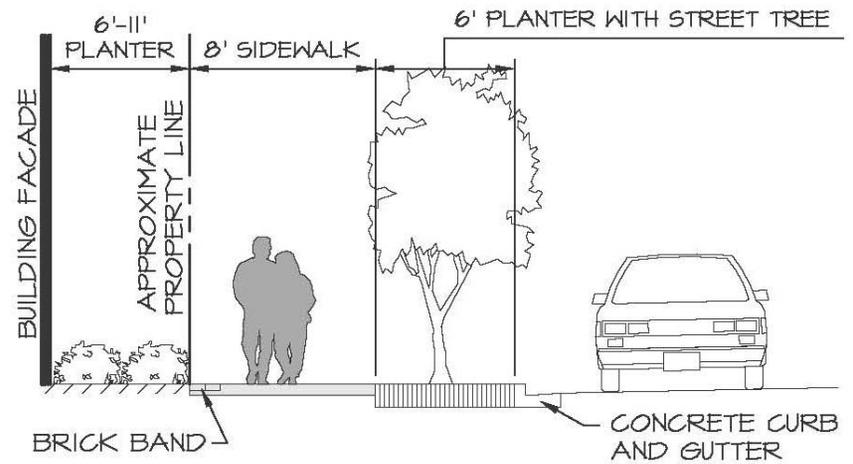
Note: Final location of site furnishings, planters and brick bands to be determined at site plan approval.  
Plan illustrates the minimum level of brick banding proposed.



## Re-Aligned Crabtree Valley Avenue Sidewalk Sections



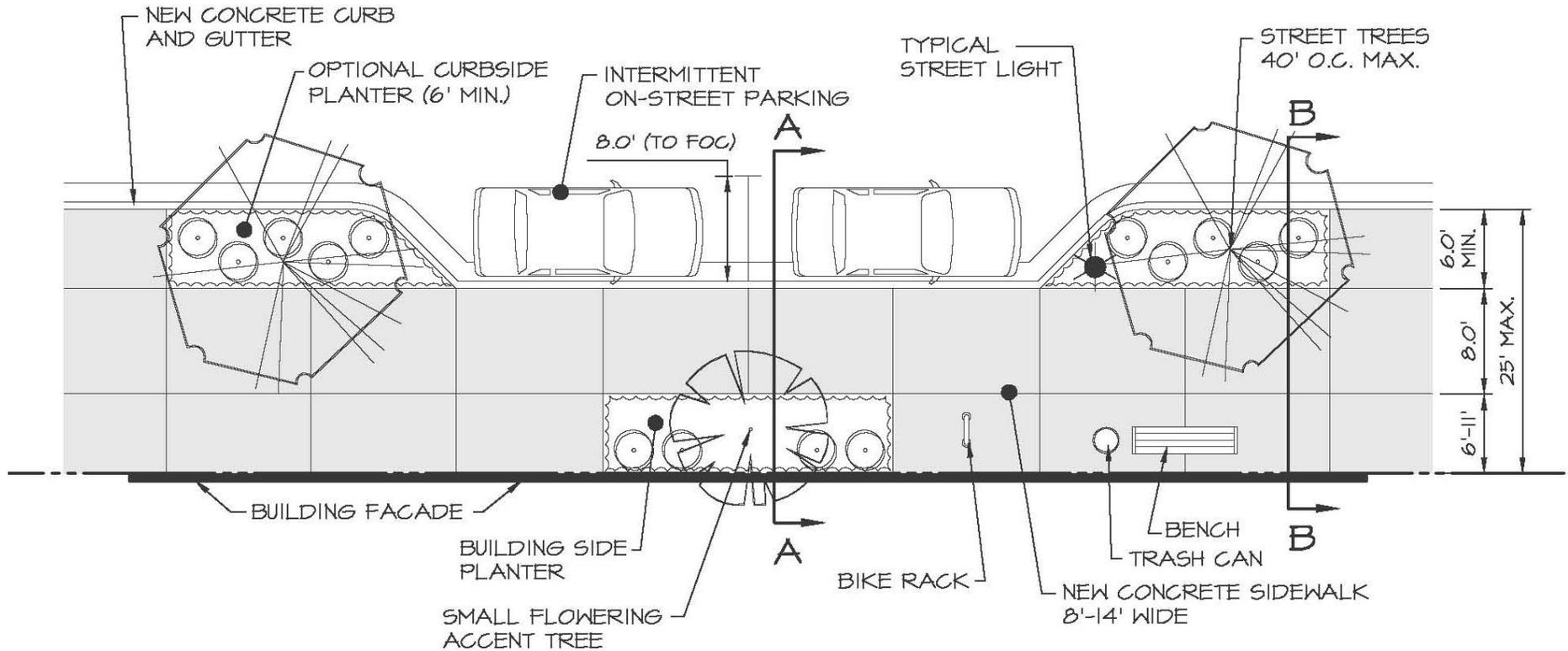
SECTION A



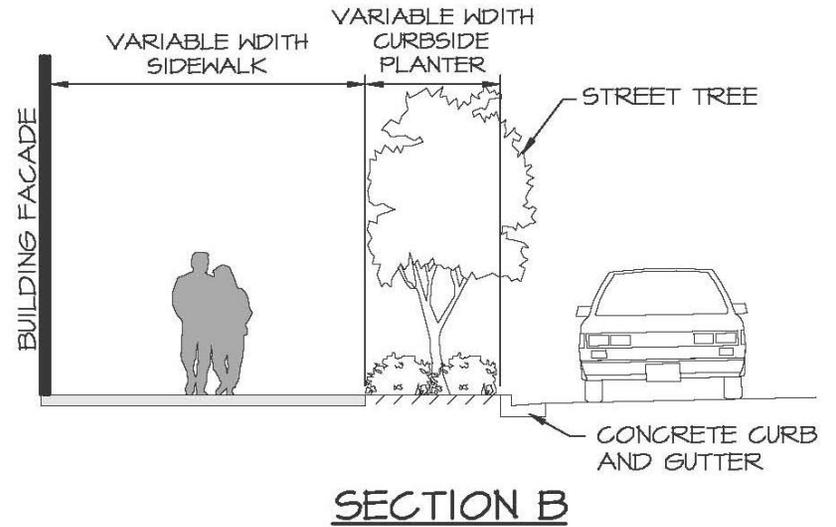
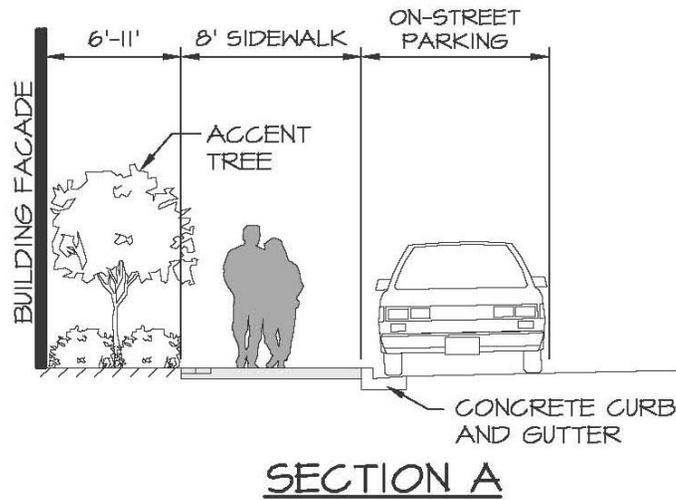
SECTION B

# Crabtree Valley Mall Connector Road Sidewalk Plan

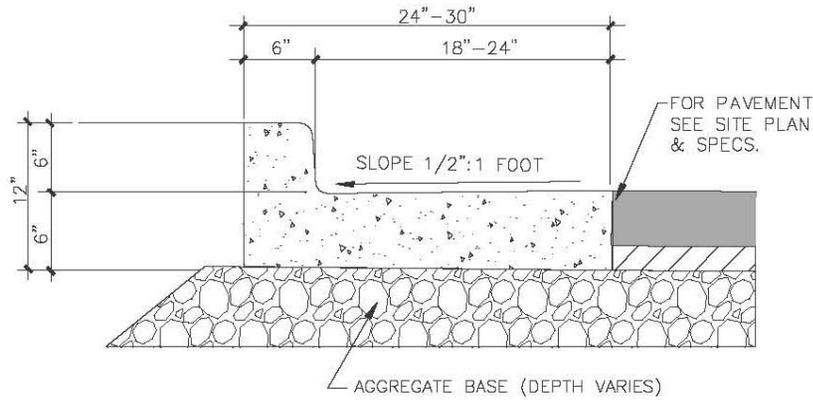
Note: Final location of planters and site furnishings to be determined at site plan approval.



# Crabtree Valley Mall Connector Road Sidewalk Sections



### Curb Detail



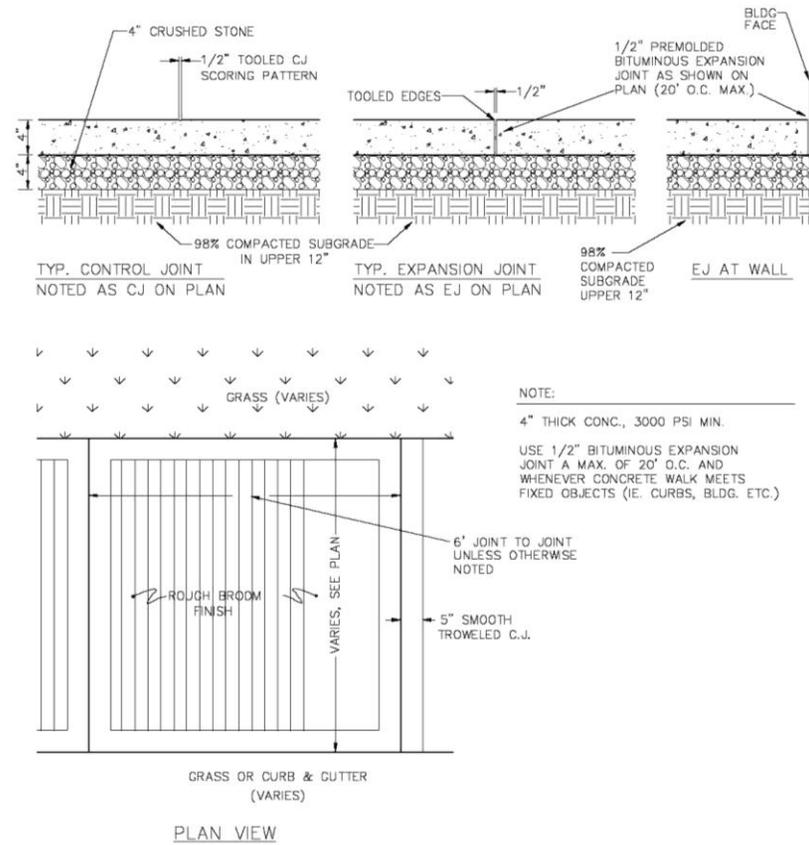
### Accent Paver Detail

**Manufacturer:** Pine Hall Brick

**Specification:** Cocoa Full Range (or equal as approved by staff at the time of site plan approval).



### Sidewalk Details



## **Streetscape Standards Continued**

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### **Utilities**

Utilities are currently in place in the area. Subject to approval by the City of Raleigh and the utility provider, all utilities will be either located underground and behind the back of curb or on poles in the right of way.

### **Maintenance Plan**

A maintenance plan will be enforced as part of the PBOD for the care of the streetscape improvements up to the public right-of-way. Maintenance in excess of that customarily provided by the municipality within public right-of-ways will require an encroachment agreement and will be the responsibility of the individual property owners. This includes maintenance of sidewalks and planters and street furnishings required by the PBOD but within the property lines, watering of plant materials and keeping the sidewalks free of debris and trash. A draft of the maintenance plan will be submitted at the time of site plan approval.

### **Signage**

The design and location of signage will compliment the architectural styles of the structures and will be oriented toward vehicular and pedestrian traffic. The following signage types are encouraged: Awning signs, window stencils and wall signs. Low profile ground signs will also be allowed on the property in accordance with the signage code. Paddle signs oriented toward pedestrian traffic will also be permitted in accordance with the signage code and shall maintain a clear minimum height of 9'-0" above the ground.

### **Canopies and Awnings**

In the event canopies or awnings are incorporated along the streetscape; they will be of sufficient height to accommodate the free flow of pedestrian traffic. In no case will the awnings be less than 9'-0" above the ground.

### **Street Lights**

Appropriate light levels along the streetscapes and parking areas are critical in creating a safe and inviting environment for pedestrians. The project will provide a mix of low level lighting as well as pedestrian scale lighting similar to the illustrations provided below. A lighting plan will be provided at the time of site plan approval.

## Pedestrian Lighting

Manufacturer: Landscapeforms

Specification: Alcott 12' Pedestrian light  
(or approved equal)

## Low Level Lighting

Manufacturer: Landscapeform

Specification: Alcott 12' Pedestrian light  
(or approved equal)



## **Building Facades**

Structures will address the public and private rights of ways. Ground level residential units will have one or more of the following elements: balconies, porches, stoops or similar active elements that address the street in order to help mitigate the interface between the proposed residential structure and the existing residential to the south.

Along the streetscape, the facades will be broken up to provide a pedestrian scale experience by the interspersions of street trees, street furnishings, decorative lighting, movement in the sidewalk, changes in paving material as well as other similar elements.

## **Building Setbacks**

Building setbacks from street right of ways within the PBOD shall be a minimum of 0' and the south and east frontages of the site will accommodate a 14' wide pedestrian walkway as shown on the sidewalk plans and sections on pages 5-10.

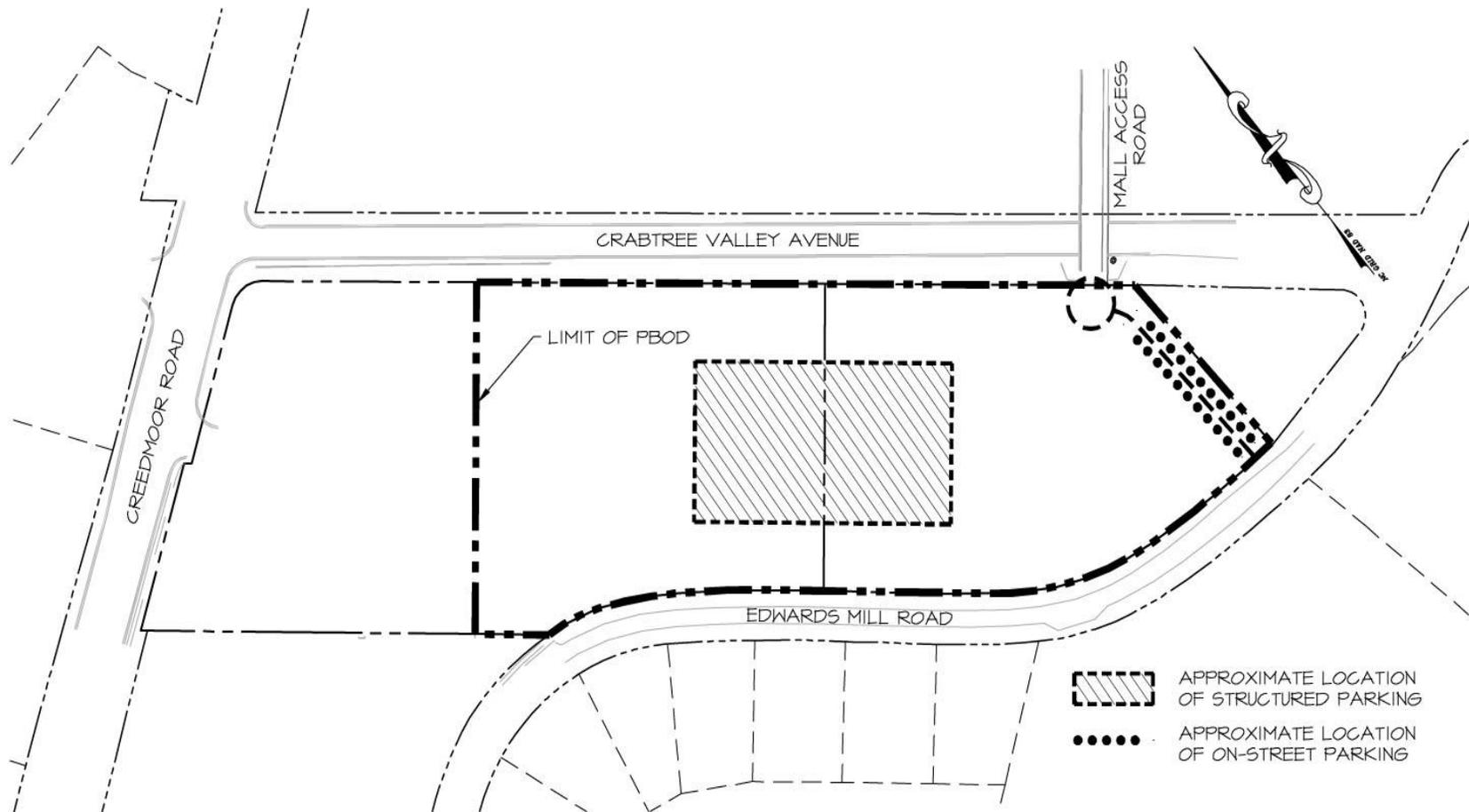
## **Building Height**

The current PDD zoning of this site allows for a maximum building height of 132' or eight stories. We are proposing to further limit the maximum building height within the PBOD overlay to 120' measured in accordance with the City of Raleigh code of ordinance. It is also important to note that the relationship of the site to the residential property to the south is greatly mitigated by the fact that Edwards Mill Road rises to 25' above the site along this frontage.

## **Parking**

The project will provide both on-street and off-street parking. Parking will be provided through a combination of both surface and structured parking. It is anticipated that the majority of the parking within the Crabtree Place PBOD will be contained in multi-level structures. The project will meet or exceed the minimum parking requirements of the City of Raleigh. Any surface parking (other than drop-off, delivery and temporary parking areas) visible from a public street will be screened from view in accordance with City standards. Bicycle parking will be provided at a rate of 1/20 car spaces unless a higher standard is required by the Code and will be located within the parking structures or near the main entrances to the structure.

## Parking Location Diagram



*Note: The actual location of the "Crabtree Valley Mall Connector Road" and associated on-street parking shown on pages 4 and 16 of this Streetscape and Parking Plan will be established at the time of site plan approval*

## Proposed Street Tree Palette

Street trees will be installed per the streetscape plan at the time of development of the parcels at the public right of ways prior to the issuance of a certificate of occupancy for the development of the parcel. Trees will be installed at a minimum size of 3" caliper. Alternate similar street tree species may be substituted at the time of site plan as approved by staff to accommodate availability of species and direction from the City of Raleigh urban forester.

### Japanese Zelkova

(Zelkova serrata)

Shape: Urn Shaped

Foliage: Green with sawtooth margins

Fall Color: Yellow to dark red



### Chinese Pistache

(Pistacia Chinensis)

Shape: Oval

Foliage: Dark green leaflets

Fall Color: Orange to red



**Columnar European Hornbeam**

(*Carpinus betulus fastigiata*)

Shape: Dense pyramidal form

Foliage: 2-5" clean leaves

Fall Color: Yellow to dark red



**Willow Oak**

(*Quercus phellos*)

Shape: Oval to spreading

Foliage: Long willow-like thin leaves

Fall Color: Yellow or russet red

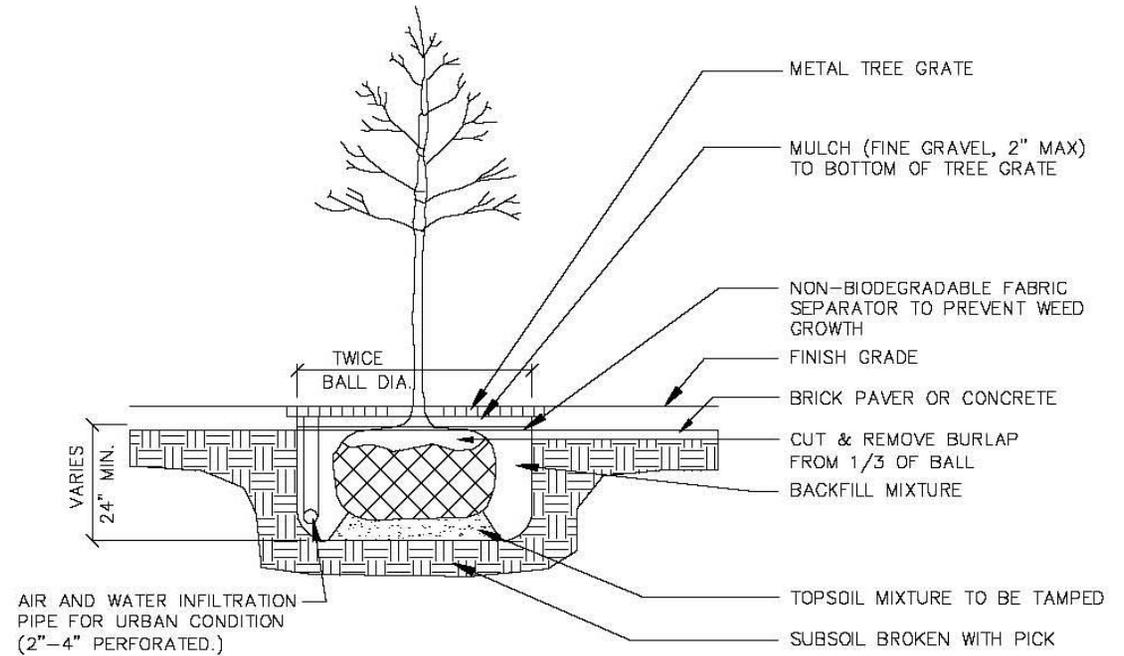


## Tree Grates

Manufacturer: Neenah foundry Products  
Specification: Adirondack Collection  
(or approved equal)



## Tree Planting Detail



## Benches

Manufacturer: Landscape Forms  
Specification: Austin Series bench with arms  
(or approved equal)

## Trash Receptacles

Manufacturer: Landscape Forms  
Specification: Austin Series litter receptacle  
(or approved equal)



## Bike Racks

Manufacturer: Landscape Forms

Specification: Bola Series bike rack  
(or approved equal)



*Note: Certain items are specified in this Plan with the phrase “(or approved equal).” At the time of site plan approval, for these items staff may allow a substitution of another manufacturer or design upon a determination that the item substituted is of equivalent quality and function.*

**Appendix – City Code Section 10-2055 – Pedestrian Business Overlay District**

**Front yard setback:** Minimum of ten (10) feet; maximum of twenty-five (25) feet.

**Side yard setback:** Minimum of zero (0) feet when minimum building separation is met.

**Building separation:** Minimum of ten (10) feet.

**Maximum building height:** Thirty-five (35) feet.

(Ord. No. 2008-428-TC-313, §2, TC-4-08, 7-1-08; Ord. No. 2010-771-TC-336, §§1—3, TC-7-10, 8-3-10)

**Sec. 10-2055.**

**PEDESTRIAN BUSINESS OVERLAY DISTRICT.**

**(a) Approval.**

If the use requires a *site plan*, as set forth in §10-2132.2, approval of a *site plan* is required by either the administrative staff, the Planning Commission, or the *City Council*; see §§10-2132.2(b) and (c). If the use requires a *plot plan*, as set forth in §10-2132.1, administrative approval is required. The required *site plan* is to be reviewed based on the procedures and standards contained in §10-2132.2(c) and (d).

(Ord. No. 1992-111-TC-42, §20, TC-19-92, 12-8-92; Ord. No. 1997-137-TC-153, §31, TC-18-96, 6-17-97; Ord. No. 2010-706-TC-331, §10, TC-1-10, 2-16-10)

**(b) Permitted Uses.**

Subject to the provisions of §10-2055 and other overlay districts, all general uses, conditional uses and special uses that are allowed in the underlying district by the **Schedule of Permitted Uses in Zoning Districts** §10-2071 are allowed in the Pedestrian Business Overlay District.

Following the approval of a *site plan* by the *City Council*, in accordance with §10-2051(d)(1), additional residential density, conversions of *building(s)* to *dwelling(s)*, reductions *net lot area* for *dwellings*, *dwellings* and *lodging units* with two (2) burner cook tops, and housing within the underlying industrial zoning districts are all permitted.

(Ord. No. 1992-88-TC-401, §13, TC-4-92, 11-4-92; Ord. No. 1997-137-TC-153, §35, TC-18-96, 6-17-97; Ord. No. 2007-262-TC-302, §10, TC-2(A.2)-07, 7-10-07)

**(c) Prohibited Uses.**

Except for improvements made pursuant to Part 10 chapter 3 of this Code and *residences* allowed in Industrial districts by the special use permit authorized by this overlay district, any use not explicitly allowed by both the underlying district by the **Schedule of Permitted Land Uses in Zoning Districts** §10-2071 and subsection (b) above is prohibited. If a use is prohibited in either an overlay district or the underly-

ing district, that use is prohibited even if one (1) of these districts allows the use. The enumeration of expressly prohibited uses *shall* not be deemed exclusive or all-inclusive. Prohibited uses include:

- All uses prohibited in the underlying zoning district
- Improvements to the sides of *buildings* fronting on a *thoroughfare* or collector *streets* to *street* rights-of-way except in conformance with the terms, conditions, plans, and provisions of the Streetscape Plan or Streetscape and Parking Plan unless approved by the Board of Adjustment in accordance with §10-2146.3(a)(7)
- Improvements to *street* rights-of-way except in conformance with the terms, conditions, plans, and provisions of the Streetscape Plan or Streetscape and Parking Plan unless approved by the Board of Adjustment in accordance with §10-2146.3(a)(7)

- *Vehicular display areas*

(Ord. No. 1992-88-TC-401, §10, TC-4-92, 11-4-92; Ord. No. 2000-740-TC-190, TC-1-99, §1, 2-18-00; Ord. No. 2004-596-TC-245, §3, TC-14-03, 4-6-04)

**(d) Area, Density, Bulk, Yard, and Height Requirements.**

**(1) Area, density, and bulk.**

- a. The required minimum *net lot area* for any *dwelling unit* and *equivalent dwelling unit* is that of the underlying zoning district unless a *site plan* is approved in accordance with §10-2051(d).
- b. The maximum residential density per net acre is that of the underlying zoning district unless a *site plan* is approved in accordance with §10-2051(d).

All minimum *net lot areas* and residential densities *shall* be calculated in accordance with §10-2073(c) and are subject to the exceptions listed in §10-2073(d).

(Ord. No. 2005-932-TC-275, §10, TC-6-05, 11-15-05)

**(2) Yard.**

The minimum overlay district yard setbacks, unless otherwise required by this Code, are:

---

<i>front yard, corner lot side yard, and rear yard that adjoin a street right-of-way</i>	-0- provided widths of pedestrian ways required in the Overlay District are met
--	---

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all other yards	same as underlying zoning district
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See §10-2075 for other *yard areas* required by this Code, method of calculating, exceptions and reductions to *yard areas*, and illustrations.

Cross reference: Required width of pedestrian ways §10-2055(e)(5)

**(3) Height.**

*Buildings* and structures *may* be constructed to any height established in the Streetscape Plan or Streetscape and Parking Plan. If the height regulations of the underlying district conflict with the height regulations of the Streetscape Plan or Streetscape and Parking Plan, the Plan *shall* control. Whenever the Streetscape Plan or Streetscape and Parking Plan fails to adopt height limitations, those of the underlying zoning district *shall* be applied to the *property*.

See §10-2075 for method of height calculation, exceptions, and illustrations.

(Ord. No. 2000-740-TC-190, TC-1-99, §1, 2-18-00)

**(4) Required open space.**

For the purposes of this subsection, and subsection 10-2051(d)(1)c.6., above, "open space" *shall* include greenways and any common outdoor landscaped and recreation spaces (excluding *vehicular surface areas* ), outdoor decks, roof gardens and other similar outdoor community space accessible to and available for use by all residents, employees and/or visitors of the development.

The minimum "open space" provided on the site *shall* be five (5) per cent of the total land area of the development, excluding dedicated rights-of-way unless density transfer is allowed from the right-of-way.

The minimum required open space area *shall* be required to conform to the following standards:

a. Sidewalk widening.

Where the property lies adjacent to a public *street* right-of-way with an existing sidewalk width of eighteen (18) or less feet, sidewalk widening *shall* be required based on the following:

<u>Existing walkway condition</u>	<u>Construct to a minimum of</u>
0—14 feet	14 feet
>14—18 feet	18 feet
>18 feet	No sidewalk construction required

Additional right-of-way dedication *shall* not be required to accommodate the sidewalk

widening set forth herein. However, for any sidewalk widening provided to comply with these minimum requirements and located on private property, the property owner *shall* be required to grant to the *City* a public access easement. The newly constructed sidewalk *shall* be of compatible paving materials with the public sidewalk and *shall* conform to any applicable adopted streetscape plan. The sidewalk widening *shall* occur along the entirety of the public *street frontage* with no permanent obstructions, other than those that *may* be required by the *City* Code and structural columns supporting overhangs or upper-story structures creating an arcade or recessed covered area in which case the columns *shall* be spaced a minimum of five (5) feet from both the building facade and the individual columns; and

b. Courtyard, roof garden, dining, recreation space.

One-half (1/2) of the required open space *shall* be in one (1) continuous part with a minimum length and width dimension of twenty (20) feet.

Street level open spaces proposed to meet the requirements of subsection (ii) above located adjacent to proposed sidewalk widening *may* utilize the portion of the proposed sidewalk located on private property to meet the minimum twenty (20) foot dimension stated above. In the event that required sidewalk widening located on private property represents greater than two and one-half (2 1/2) percent of the total land area of the development, the minimum percentage requirement established by subsection (ii) shall be reduced accordingly such that no more than five (5) per cent of the total land area of the development *shall* be required to be devoted to open space required by this §10-2051(e).

No open space *shall* be required for any of the following:

- Rehabilitation of buildings or portions of buildings to sixteen (16) or fewer *dwelling units* or *congregate care* or *congregate living units*, per building.
- *Single family* detached *dwellings* or *duplex dwellings* on their own *lot*, where "open space" equivalent to the greater of twenty (20) per cent of the area of the *lot* or four

hundred thirty-five and six-tenths (435.6) square feet per *dwelling* or *rooming unit* is provided.

For rehabilitation of buildings or portions of buildings for all other uses, the designated approving authority as specified by §10-2132.2(c) *may* approve an alternate method of compliance for the required open space. The approved alternate method of compliance *may* be less than five (5) per cent of the total land area of the development. Alternate methods of compliance include, but are not limited to, private balconies and decks, indoor recreation areas, meeting rooms and community space accessible to and available for use by the residents, employees and/or visitors.

The minimum open space required by this Code for developments located in a Pedestrian Business Overlay District *may* be reduced as part of the subdivision or *site plan* approved by the Planning Commission or the *City Council* in accordance with §10-2132.2 after a finding that such reduction in open space is in accordance with the general plans for the physical development of the *City* as embodied in the Comprehensive Plan and after a finding that the *site plan* complies with the procedures and standards of §10-2132.2(c) and (d).

**Editor's note:** Prior to December 1, 2007, all high density residential projects approved in accordance with §10-2051(d)(1) *shall* be required to provide the following minimum "open space" requirement:

The minimum "open space" provided on the site *shall* be fifteen (15) per cent of the total land area of the development, excluding dedicated rights-of-way unless density transfer is allowed from the right-of-way, but not to exceed four hundred thirty-five and six-tenths (435.6) square feet per unit or in the case of *congregate care* or *congregate living structures*, not to exceed two hundred eighteen (218) square feet per *dwelling* or *rooming unit*. In no case *shall* less than ten (10) per cent of the total land area of the development, excluding dedicated rights-of-way unless density transfer is allowed from that right-of-way, be devoted to common outdoor open space accessible to all residents.

**Cross reference:** Open space is required for any *cluster unit development*, *congregate care structure* or *congregate living structure*, *group housing development*, *life care community*, *manufactured home park*, *multi-family dwelling development*, *residential unit ownership (condominiums)* and *townhouse development*, Part 10, chapter 2, Article F. (Ord. No. 1992-88-TC-401, §24, TC-4-92, 11-4-92; Ord. No. 1997-137-TC-153, §36, TC-18-96, 6-17-97; Ord. No. 2008-450-TC-316, §4, TC-11-08, 9-2-08; Ord. No. 2010-706-TC-331, §§11, 12, TC-1-10, 2-16-10)

#### (e) Supplementary Regulations.

All uses and activities are also subject to Article H, overlay zoning districts, conditional use zoning districts, and supplementary regulations of Article E. Supplementary regulations include:

#### (1) Off-street parking.

The minimum number of off-street parking spaces required for commercial and recreational land uses *shall* be based upon the ratios and coordinating strategies recommended by the adopted Streetscape and Parking Plan for the subject area of the Overlay District subject to compliance with the construction improvements required by the Streetscape and Parking Plan. In no case *shall* a Streetscape and Parking Plan adopt parking strategies which are less restrictive than that specified below. Where a Streetscape Plan had previously been adopted, but no Streetscape and Parking Plan has been subsequently adopted, the minimum number of off-street parking spaces *shall* be the number of spaces required below subject to compliance with the construction improvements required by the Streetscape Plan.

Except for *site plans* approved pursuant to Section 10-2051(d)(1) or for projects in the Transit Orientated Development Overlay District, the minimum number of off-street parking spaces required for new structures, additions or expansions to existing structures or changes in use *shall* be one (1) space per four hundred (400) square feet of *building floor area* or the minimum number of parking spaces set forth in §10-2081(a), SCHEDULE OF OFF-STREET PARKING STANDARDS, whichever is less, with the exception of the following land uses:

- a. Projects with sixteen (16) or fewer *dwelling units* per *building* *shall* not be required to provide any off-street parking spaces for the *dwelling units*. Projects with seventeen (17) or more *dwelling units* per *building* *shall* not be required to provide off-street spaces for the first sixteen (16) *dwelling units*, but *shall* be required to provide at least one (1) off-street parking space per unit in excess of sixteen (16) units; with the exception that, units occupied by no more than two (2) residents not related by blood, marriage or adoption sixty-two (62) years and older *shall* provide a minimum of one-half (1/2) space per unit. For residential developments or the residential portion of a mixed-use development, no more than two (2) parking spaces per *dwelling unit* *shall* be located on the site. However, proposed expansions or modifications to existing developments with on-site parking *shall* be required to retain a minimum of one (1) on-site parking space devoted to persons with disabilities.

- b. No parking *shall* be required for retail facilities located within existing uses; new structures, additions or expansions to existing structures or changes in use, all ten thousand (10,000) square feet or less; or, the first ten thousand (10,000) square feet of existing uses, new structures, additions or expansions to existing structures or changes in use, all greater than ten thousand (10,000) square feet. For purposes of this subsection, "retail facilities" means *retail sales- convenience, retail sales-personal services, food store-retail, recreational indoor use - commercial, retail banking; and eating establishments, bars, nightclubs, taverns and lounges* located no closer than one hundred (100) feet to a residential zoning district or located within one hundred (100) feet of a residential zoning district with hours of operation not extending past 11:00 p.m. However, proposed expansions or modifications to existing developments with on-site parking *shall* be required to retain a minimum of one (1) on-site parking space devoted to persons with disabilities.
- c. Parking requirements for *eating establishments, bars, nightclubs, taverns and lounges*

located within one hundred (100) feet of a residential zoning district with hours of operation extending past 11:00 p.m. *shall* be that specified by the adopted Streetscape and Parking Plan for commercial uses. Where a Streetscape Plan had previously been adopted, but no Streetscape and Parking Plan has been subsequently adopted, the minimum number of off-street parking spaces *shall* be one space for every one hundred (100) square feet *floor area gross of building* for public use or one space for every eight (8) seats, whichever is greater.

(Ord. No. 2000-740-TC-190, TC-1-99, §2, 2-18-00; Ord. No. 2008-350-TC-307, §3, TC-2(B)-07, 2-5-08)

**(2) Vehicular parking areas.**

No new vehicular parking areas, including additions to vehicular parking areas existing prior to the application of the Pedestrian Business Overlay District, *shall* be located in any portion of the site parallel to and adjoining a *thoroughfare* unless such vehicular parking areas are located behind the front wall of the *principal building* as viewed perpendicular to the recorded *thoroughfare street right-of-way*.

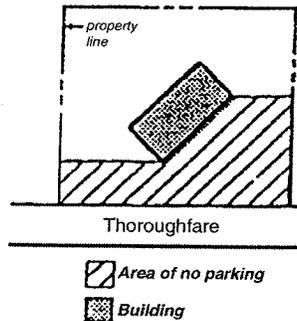


Diagram: No Parking Area Relative to Thoroughfare

Vehicular parking areas lawfully existing prior to the application of the Pedestrian Business Overlay District *may* be utilized by existing uses and *buildings* in the Overlay District. These vehicular parking areas *shall* be made to conform to the adopted Streetscape Plan or Streetscape and Parking Plan, the design standards of §10-2081(b), and the provisions for handicap spaces whenever those

*buildings* and uses that utilize these vehicular parking spaces undergo one (1) or more of the following:

- a. Expansions or additions that singularly or collectively exceed either twenty-five (25) per cent of the total *floor area gross* of the *building* or twenty-five (25) per cent of the total gross area occupied by the use where

there is no principal *building*. The per cent of expansion is to be determined with reference to the size and area of the *building* or use which existed at the time the Overlay District first became applicable to the *property*.

- b. Renovation or repair work which, during any one (1) calendar *year* exceeds twenty-five (25) per cent of the Wake County listed tax value.
- c. Change in use which results in a change in the type of **Building Code** occupancy as set forth in the **North Carolina Building Code**.

(Ord. No. 2000-740-TC-190, TC-1-99, §3, 2-18-00)

**(3) Landscape.**

Landscaping *shall* be done in accordance with §10-2082, and all *street trees shall* be planted in accordance with the adopted Streetscape Plan or Streetscape and Parking Plan.

(Ord. No. 2000-740-TC-190, TC-1-99, §3, 2-18-00)

**(4) Signage.**

See §10-2083 for *on-premise signs* and §10-2084 for *off-premise signs*. No *sign shall* be erected, altered, replaced, restored, or moved unless the change is in accordance with the Streetscape Plan or Streetscape and Parking Plan.

(Ord. No. 2000-740-TC-190, TC-1-99, §3, 2-18-00)

**(5) Pedestrian ways.**

The minimum width of pedestrian ways from the curb to the *building* line for all new *buildings* and uses *shall* be eighteen (18) feet wide, unless the width of pedestrian ways for more than seventy-five (75) per cent of the linear frontage of pedestrian ways on the same side of the *street* as the new development, within a single block, are less than fourteen (14) feet. In such instances, the minimum pedestrian way *shall* be in accordance with §10-2055(d)(4) above.

All improvements to pedestrian ways *shall* be made in accordance with the adopted Streetscape Plan or Streetscape and Parking Plan. Upon adoption of a Streetscape and Parking Plan, alternatives to the aforementioned requirements for pedestrian ways *may* be established by the adopted Streetscape and Parking Plan. All improvements to pedestrian ways *shall* be made in accordance with the adopted Streetscape and Parking Plan whenever the *property* is developed using any of the less stringent standards of this Overlay District.

(Ord. No. 2000-740-TC-190, TC-1-99, §4, 2-18-00; Ord. No. 2008-450-TC-316, §5, TC-11-08, 9-2-08)

**(6) Bicycle parking.**

Bicycle parking spaces *shall* be provided for all new uses, new *buildings*, and for existing *buildings* and uses whenever those existing *buildings* and uses undergo any one (1) or more of the expansions, expenditures, or changes listed in subsections a. through c. of §10-2055(e)(2) above. Bicycle spaces *shall* be provided at a rate of at least one (1) bicycle space for every twenty (20) automobile spaces provided of five thousand (5,000) square feet of nonresidential *floor area gross*, whichever is greater. Bicycle parking facilities *shall* be located within easy access from the *street* right-of-way, and *shall* be designed in accordance with the Streetscape Plan or Streetscape and Parking Plan.

(Ord. No. 1992-111-TC-42, §21, TC-19-92, 12-8-92; Ord. No. 2000-740-TC-190, TC-1-99, §5, 2-18-00; Ord. No. 2008-350-TC-307, §4, TC-2(B)-07, 2-5-08)

**(7) Exterior building walls.**

Fifteen (15) linear feet *shall* be the maximum length of a "blank exterior *building* wall plane" facing a *thoroughfare* for any new *building* and for any existing *building* that is either replaced or undergoes one (1) or more of the expansions, expenditures, or changes listed in subsections a. through c. of §10-2055(e)(2) above. All other existing *buildings shall* not increase the amount of "blank exterior *building* wall plane" beyond that which is allowed for new *buildings*. A "blank exterior *building* wall plane" is any wall unbroken to the height of nine (9) feet by any one (1) or more of the *following*:

- a. A public doorway made of transparent materials.
- b. A doorway made of opaque materials and recessed at least three (3) feet.
- c. A stairway directly available at *street* level to the public, but not fire escapes or false stairways.
- d. A window or fenestration opening in keeping with the architectural character of the surrounding area of at least twelve (12) square feet in area and no more than four (4) feet above the sidewalk at its lowest point.

(Ord. No. 2000-740-TC-190, TC-1-99, §6, 2-18-00)

**(8) Thoroughfare driveway widths.**

The maximum length of any curb cut made to a *thoroughfare* after application of the Pedestrian

Business Overlay District *shall* be fifteen (15) feet for one-way driveways and twenty-five (25) feet for two-way driveways or joint driveways.

**(9) "Streetscape Plan" and "Streetscape and Parking Plan."**

No improvement to *street* rights-of-way, to *vehicular surface areas* and to the sides of any *building* that face a *thoroughfare* or a collector *street shall* be made within a Pedestrian Business Overlay District except in conformity with the adopted Streetscape Plan or Streetscape and Parking Plan. All additions, changes, expansions, and alterations to such existing structures must comply with the regulations of the Streetscape Plan or Streetscape and Parking Plan unless the Board of Adjustment in accordance with §10-2146.3(a)(7) approves the addition, change, expansion, or alteration, as if the structure were made nonconforming by the Overlay District. Improvements to *thoroughfare* rights-of-way that are part of the Streetscape Plan or Streetscape and Parking Plan *shall* be installed whenever the *properties* that adjoin said rights-of-way acquire new *buildings*, new uses, or undergo expansions, expenditures, or changes listed in subsections a. through c. of §10-2055(e)(2) above. These required improvements to *street* rights-of-way *shall* be completed prior to the issuance of a *building* occupancy certificate.

(Ord. No. 2000-740-TC-190, TC-1-99, §7, 2-18-00; Ord. No. 2003-373-TC-231, §5, TC-23-02, 2-4-03)

**(10) Existing structures and lots.**

A structure existing at the time that the Pedestrian Business Overlay District is first applied to the *property shall* not be deemed a *zoning nonconformity* solely because of this Overlay District. All additions, changes, expansions, and alterations must comply with the Streetscape Plan or Streetscape and Parking Plan, unless the Board of Adjustment in accordance with §10-2146.3(a)(7) approves the addition, change, expansion, or alteration as if the structure or use were made nonconforming by the Overlay District.

(Ord. No. 2000-740-TC-190, TC-1-99, §7, 2-18-00; Ord. No. 2003-373-TC-231, §5, TC-23-02, 2-4-03)

**(f) Designation of a Pedestrian Business Overlay District and Adoption of a Streetscape and Parking Plan.**

**(1) Locational guidelines.**

The general characteristics desired of land placed in the Pedestrian Business Overlay District are the following:

- a. It consists primarily of *retail uses* as defined in §10-8002.
- b. It comprises at least one (1) side of one (1) block, including any existing Pedestrian Business Overlay District.
- c. It draws pedestrian patronage to it from an adjoining employment center or a residential neighborhood.
- d. It is an area with one (1) or more of the following characteristics:
  1. It was developed prior to the application of off-*street* parking requirements; or
  2. It possesses unifying distinctive *built environmental characteristics* that create an identifiable pedestrian setting, character or association;
  3. It is an expansion of an existing Pedestrian Business Overlay District.

**(2) Streetscape and Parking Plan.**

A Streetscape and Parking Plan *shall* be completed for any area zoned a Pedestrian Business Overlay District. No Streetscape and Parking Plan *shall* be approved by the *City Council* until after a recommendation thereon has been made by the Appearance Commission. At least ten (10) days before the Planning Commission makes its recommendation to the *City Council*, a copy of the Streetscape and Parking Plan *shall* be mailed to *property owners* within the proposed Pedestrian Business Overlay District. The Streetscape and Parking Plan *shall* be approved prior to or in conjunction with the zoning of any area as a Pedestrian Business Overlay District. The Streetscape and Parking Plan records designs and policies concerning the character and function of the area, and *shall* be at least one (1) block long on both sides of the block. The Streetscape and Parking Plan *shall* include at least these elements:

- a. Sidewalk building materials and typical construction detail, showing the layout pattern, underlayment, and curb design
- b. *Street* furniture types, including sidewalk light fixtures, benches, garbage receptacles, tree guards and grates, and other furnishings within the public right-of-way
- c. Canopy or awning height above sidewalk, construction type, materials, and size

- d. *Street tree species, planting detail for street trees (showing drainage, planting hole design, tree support and protection), and location of street trees in the right-of-way*
- e. Pedestrian crosswalk location
- f. Utility line location, either under the sidewalk or above it in the public right-of-way
- g. A maintenance plan for the care of streetscape improvements to the public right-of-way
- h. *Signs, types, sizes, graphics, materials and colors*
- i. Location and arrangement of public off-site and off-street *parking lots* and a plan for providing adequate public off-site and off-street vehicle and bicycle parking for the Overlay District.
- j. Assessment of the extent to which minimum off-street parking requirements *may* be reduced for commercial or recreational uses.
- k. Width of pedestrian ways from the curb(s) to the building line.
- l. Maximum height limitations.

**Editor's Note:** Prior to February 23, 2000 (effective date of Ordinance No. 2000-740-TC-190, TC-1-99), a Streetscape Plan (not a Streetscape and Parking Plan) was required to be approved in conjunction with a Pedestrian Business Overlay District. A Streetscape Plan did not include subparagraph "i." through "l." above.

(Ord. No. 1986-856-TC-274, §3, TC-11-86, 9-16-88; Ord. No. 1987-29-TC-294, §10, TC-14-87, 12-1-87; Ord. No. 1989-438-TC-344, §1, TC-11-89, 10-3-89; Ord. No. 2000-740-TC-190, TC-1-99, §8, 2-18-00)

**Sec. 10-2056.  
RESERVOIR WATERSHED PROTECTION AREA  
OVERLAY DISTRICT.**

**(a) Approval.**

If the use requires a *site plan* as set forth in §10-2132.2, approval of a *site plan* is required by either the administrative staff, the Planning Commission, or the *City Council*; see §10-2132.2(b) and (c). If the use requires a *plot plan*, as set forth in §10-2132.1, administrative approval is required.

(Ord. No. 1997-137-TC-153, §2, TC-18-96, 1-14-97)

**(b) Definitions.**

**Primary water supply watershed protection area.** The area in a Reservoir Watershed Protection Area Overlay District or an Urban Water Supply Watershed Protection Area Overlay District between the intake or mean high water level of the reservoir and the *secondary*

*water supply watershed protection area.* To protect the watershed, special standards for *impervious surface coverage, uses, natural resource buffer yards and lot sizes* are applied. *Primary water supply watershed protection areas* are delineated on the **Official Zoning Maps** and reflected in the *Comprehensive Plan*.

**Secondary water supply watershed protection area.** The area in a Reservoir Watershed Protection Overlay District or an Urban Water Supply Watershed Protection Area Overlay District between the *primary water supply watershed protection area* and surrounding *properties*. To provide a protective transition between the *primary water supply watershed protection area* and the surrounding *properties*, special standards for *impervious surface coverage, uses, and natural resource buffer yards* are applied. *Secondary water supply watershed protection areas* are delineated on the **Official Zoning Maps**, and reflected in the *Comprehensive Plan*.

(Ord. No. 2005-817-TC-267, §3, TC-7-05, 4-19-05)

**(c) Permitted Uses.**

Subject to the provisions of §10-2056 and other overlay districts, all general uses, conditional uses, and special uses that are allowed in the underlying zoning district by the **Schedule of Permitted Uses in Zoning Districts**, §10-2071, are allowed in the Reservoir Watershed Protection Area Overlay District. In the *primary water supply watershed protection area* of this Overlay District, the *following* uses are only allowed (even if the underlying zoning district classifies the use as a Board of Adjustment special use or a general use) when a special use permit is issued by the *City Council* in accordance with §10-2145 or by the Board of Adjustment if the *City* is the applicant:

- *Recreation use restricted to membership - not for profit in a residential district, Agricultural Productive, or Residential Business District.*
- Recreation use of a governmental entity.
- Not for profit recreational camps.

(Ord. No. 2005-817-TC-267, §4, TC-7-05, 4-19-05)

**(d) Prohibited Uses.**

Except for improvements made pursuant to Part 10 chapter 3 of this Code, any use not explicitly allowed in the underlying district by either the **Schedule of Permitted Land Uses in Zoning District** §10-2071 or subsection (c) above is prohibited in the Reservoir Watershed Protected Area Overlay District. If a use is prohibited in either an overlay district or the underlying district, that use is prohibited even if one (1) of

June 2, 2011

Michael D. Kaney, P.E.  
Weingarten Realty  
9420 Forum Drive, Suite 101  
Raleigh, NC 27615  
P: 919.845.3950

Subject: **Trip Generation Study for Rezoning**  
Comparison of Existing Zoning and Proposed Zoning  
Crabtree Place Development - Raleigh, North Carolina

Dear Mr. Kaney:

This letter provides a response to the City's review comments on the trip generation comparison submitted for the proposed zoning of property associated with the Crabtree Place Development. This letter provides updated trip generation calculations for the maximum build out under the existing zoning and trip generation for the maximum build out under the proposed zoning. The parcel included in the proposed rezoning is located on the east side of Creedmoor Road and south of existing Crabtree Valley Avenue. Refer to Figure 1 for the property location.

#### Existing Zoning Trip Generation

The trip generation volumes for the existing zoning were determined using the ITE Trip Generation Manual, 8<sup>th</sup> Edition. As required by City staff, land uses and densities considered allowable under existing zoning were required to be illustrated in a plan to demonstrate a possible development scenario. The existing land uses were obtained from the Weingarten Plan dated April 20, 2007. The land uses and densities shown in the April 2007 Weingarten Plan are less than the maximum allowable densities per the existing zoning. Trip generation calculations are attached.

The following assumptions were made in determining the trip generation for the existing zoning:

- The assumed potential possible development under existing zoning would allow for 474 apartment units, 150,000 square feet of office space and 273,500 square feet of retail space.
- Internal capture trips were calculated based on the ITE Trip Generation Handbook.
- Per the ITE Handbook, a pass-by rate of 34% is applied to weekday PM peak hour trips for the retail space.

Proposed Rezoning Trip Generation

The trip generation volumes for the proposed zoning were determined using the ITE Trip Generation manual, 8<sup>th</sup> Edition. Trip generation calculations are attached. These trip generation volumes were calculated under the following assumptions:

- The proposed zoning would allow for a maximum build out of 575 apartment units, 100,000 square feet of office space and 200,000 square feet of retail space.
- Internal capture trips were calculated based on the ITE Trip Generation Handbook.
- Per the ITE Handbook, a pass-by rate of 34% is applied to weekday PM peak hour trips for the retail space.

Conclusions

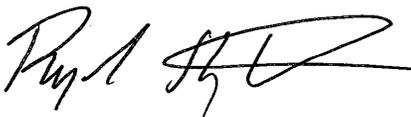
As shown in the attached spreadsheets, the total AM and PM peak hour trips calculated for the proposed zoning are lower than those allowed under the existing zoning. The AM and PM peak hour trip generations under the existing and proposed zoning are summarized below.

Existing Zoning: AM: 774 trips (445 entering, 329 exiting); PM: 1,076 trips (486 entering, 590 exiting)  
Proposed Zoning: AM: 705 trips (364 entering, 341 exiting); PM: 934 trips (444 entering, 490 exiting)

It should be noted that the existing zoning allows a maximum build out of 600 apartment units, 150,000 square feet of office space and 226,000 square feet of retail space. The proposed rezoning lowers the maximum density for each of these uses allowed under the existing zoning.

Please let us know if you have any further questions regarding this information.

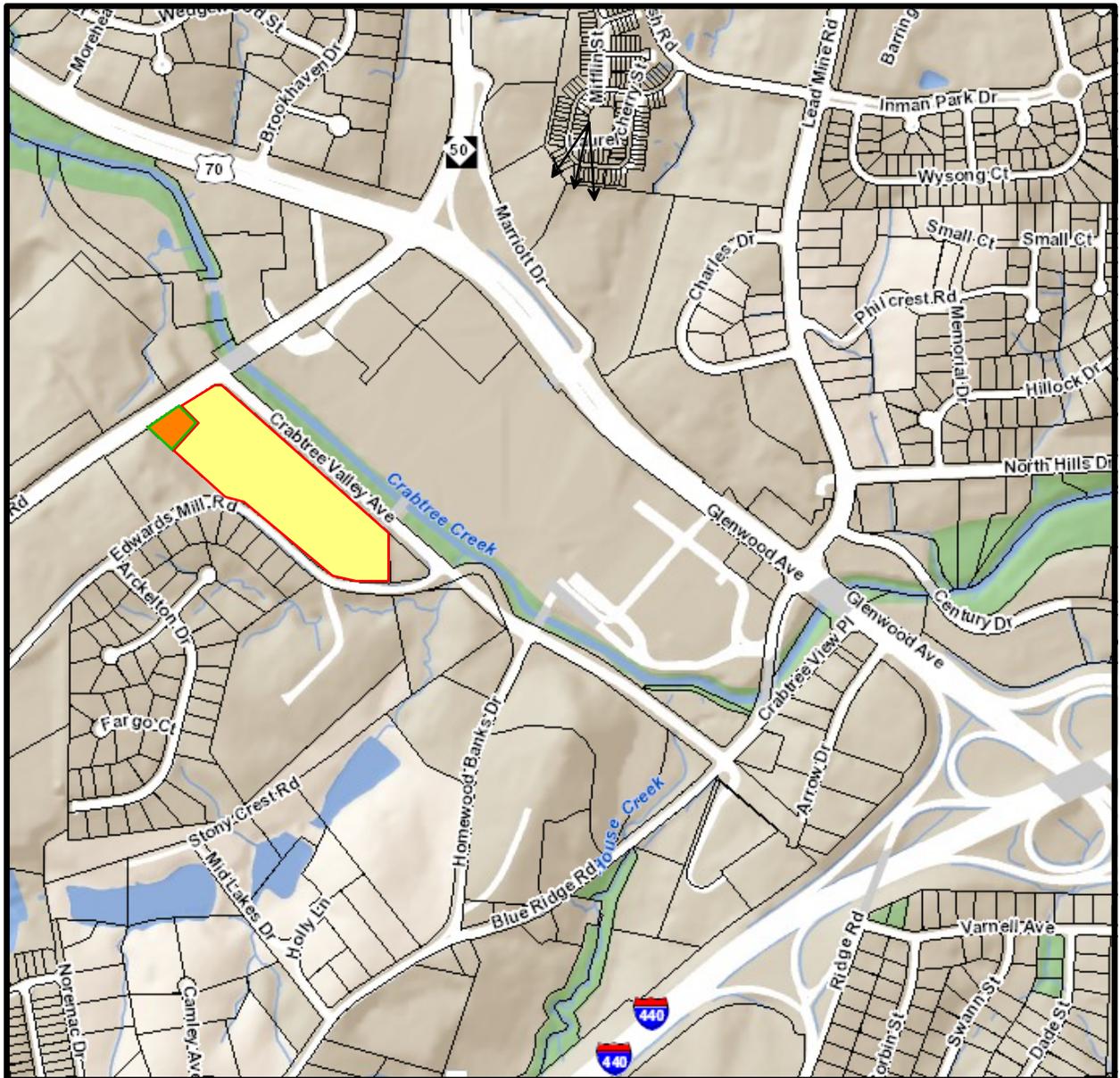
Sincerely,  
**RAMEY KEMP & ASSOCIATES, INC.**



Rynal Stephenson, P.E.  
Transportation Manager

NC Corporate License # C-0910

Attachments



**LEGEND**

- Parcel 1
- Parcel 2



 <b>RAMEY KEMP &amp; ASSOCIATES</b>	
<i>Crabtree Place Development Raleigh, North Carolina</i>	
Site Location Map	
	Figure 1

## EXISTING ZONING - POSSIBLE DEVELOPMENT

### Trip Generation (8th Edition)

Land Use		Intensity		Daily			AM Peak Hour			PM Peak Hour		
				Total	In	Out	Total	In	Out	Total	In	Out
220	Apartment	474	d.u.	2,996	1,498	1,498	236	47	189	278	181	97
710	General Office Building	150,000	s.f.	1,823	912	911	259	228	31	247	42	205
820	Shopping Center	273,500	s.f.	13,060	6,530	6,530	279	170	109	1,248	612	636
<b>Subtotal</b>				<b>17,879</b>	<b>8,940</b>	<b>8,939</b>	<b>774</b>	<b>445</b>	<b>329</b>	<b>1,773</b>	<b>835</b>	<b>938</b>
<i>Internal Capture (from ITE Worksheets)</i>				2,651	1,326	1,326	0	0	0	273	136	136
Net External Trips (Driveway Volume)				15,228	7,614	7,613	774	445	329	1,500	699	802
Pass-By Trips (34% of 820 in PM Only)							0	0	0	424	212	212
<b>TOTAL Primary Trips (New Traffic)</b>				<b>15,228</b>	<b>7,614</b>	<b>7,613</b>	<b>774</b>	<b>445</b>	<b>329</b>	<b>1,076</b>	<b>486</b>	<b>589</b>

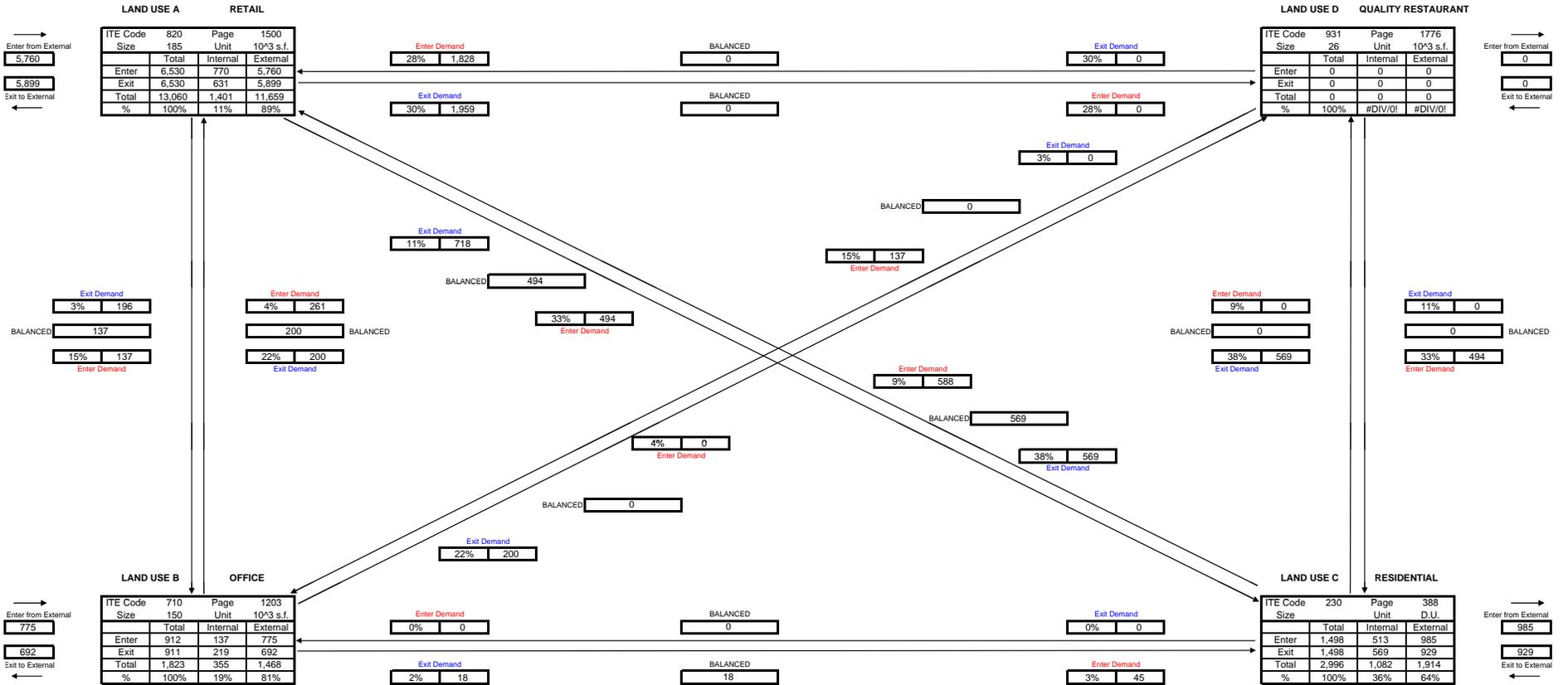
## PROPOSED REZONING

### Trip Generation (8th Edition)

Land Use		Intensity		Daily			AM Peak Hour			PM Peak Hour		
				Total	In	Out	Total	In	Out	Total	In	Out
220	Apartment	575	d.u.	3,608	1,804	1,804	285	57	228	334	217	117
710	General Office Building	100,000	s.f.	1,334	667	667	188	165	23	191	32	159
820	Shopping Center	200,000	s.f.	10,656	5,328	5,328	232	142	90	1,012	496	516
<b>Subtotal</b>				<b>15,598</b>	<b>7,799</b>	<b>7,799</b>	<b>705</b>	<b>364</b>	<b>341</b>	<b>1,537</b>	<b>745</b>	<b>792</b>
<i>Internal Capture (from ITE Worksheets)</i>				2,651	1,326	1,326	0	0	0	259	130	130
Net External Trips (Driveway Volume)				12,947	6,473	6,473	705	364	341	1,278	615	662
Pass-By Trips (34% of 820 in PM Only)							0	0	0	344	172	172
<b>TOTAL Primary Trips (New Traffic)</b>				<b>12,947</b>	<b>6,473</b>	<b>6,473</b>	<b>705</b>	<b>364</b>	<b>341</b>	<b>934</b>	<b>443</b>	<b>490</b>

EXISTING ZONING PLAN

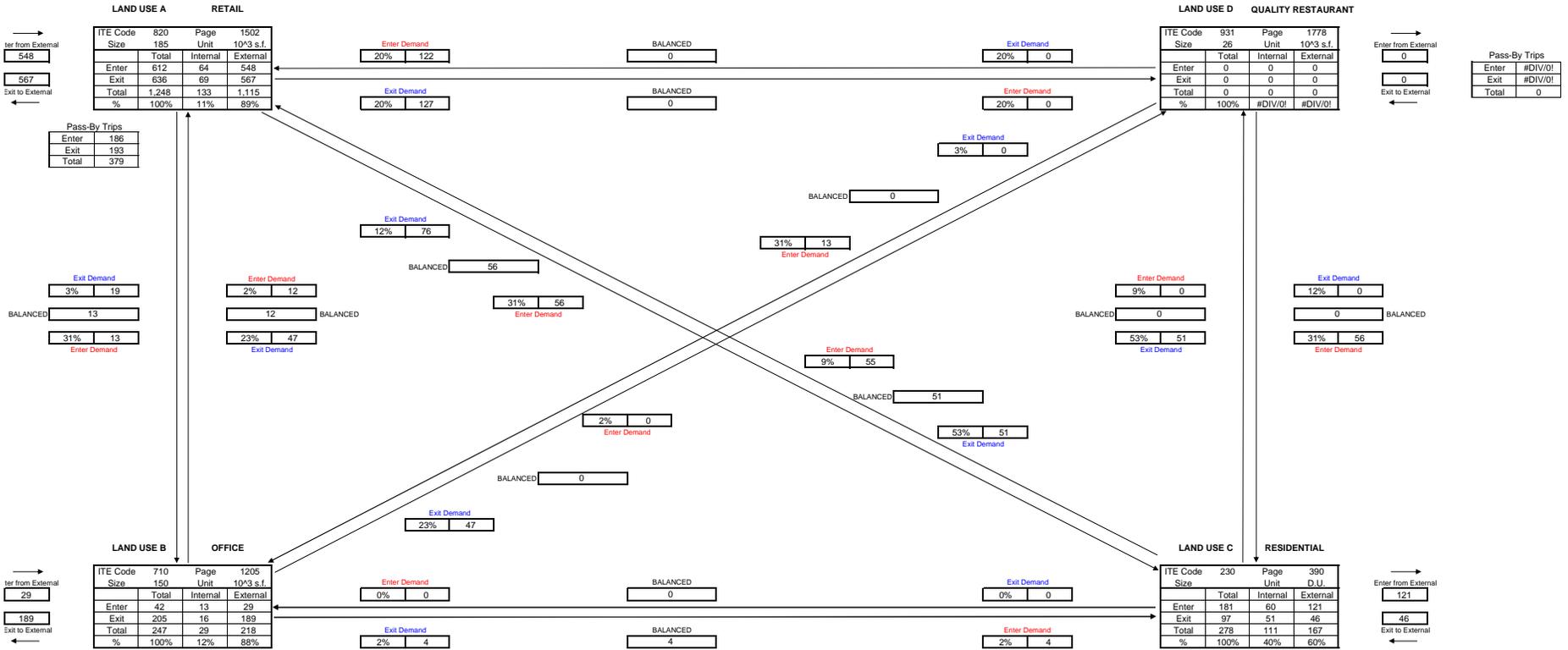
DAILY TRIPS - INTERNAL CAPTURE



NET EXTERNAL TRIPS FOR MULTI-USE DEVELOPMENT					
	RETAIL	OFFICE	RESIDENTIAL	MISCELLANEOUS	TOTAL
Enter	5,760	775	985	0	7,521
Exit	5,899	692	929	0	7,520
Total	11,659	1,468	1,914	0	15,041
Single-Use Trip Gen. Estm.	13,060	1,823	2,996	0	17,879
					INTERNAL CAPTURE %
					16%

**EXISTING ZONING PLAN**

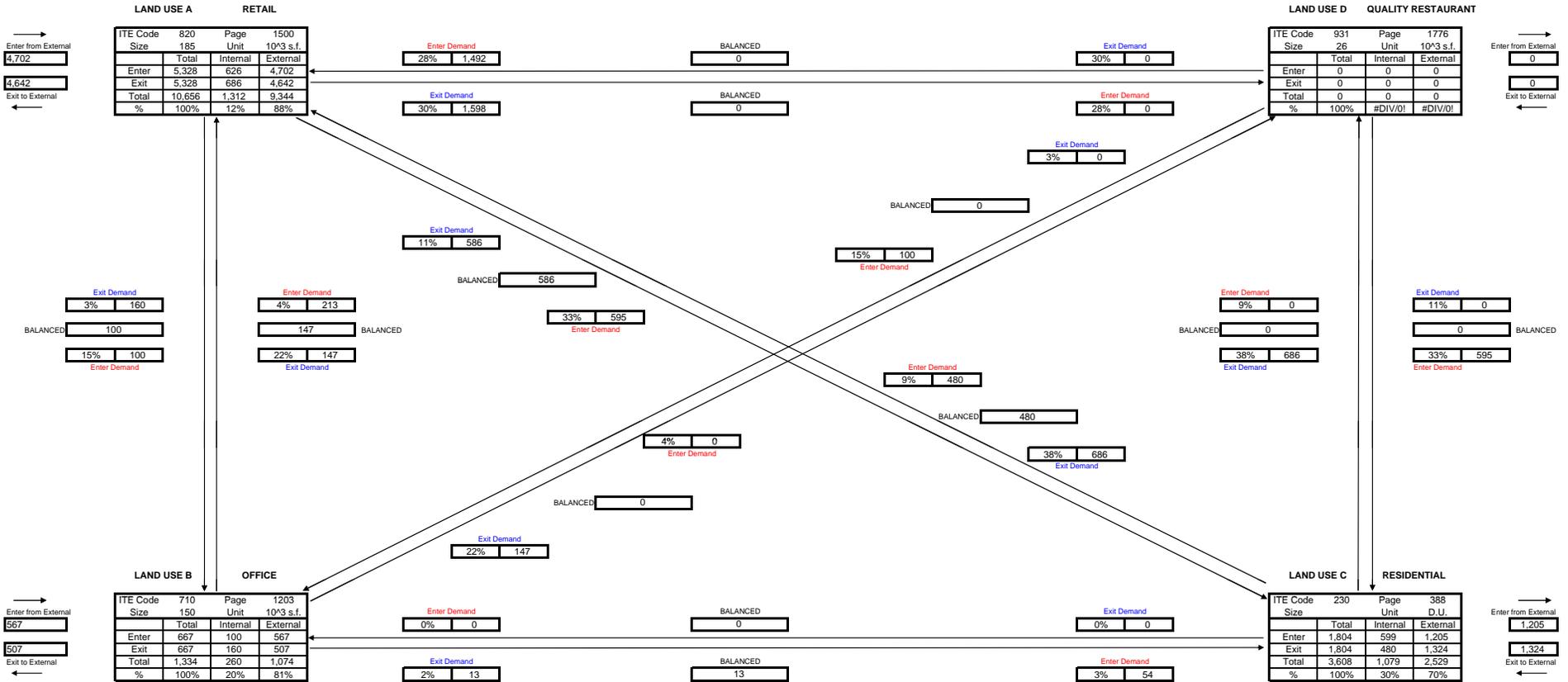
**PM Peak Hour of Adjacent Street**



NET EXTERNAL TRIPS FOR MULTI-USE DEVELOPMENT					
	RETAIL	OFFICE	RESIDENTIAL	MISCELLANEOUS	TOTAL
Enter	548	29	121	0	699
Exit	567	189	46	0	802
Total	1,115	218	167	0	1,500
Single-Use Trip Gen. Estm.	1,248	247	278	0	1,773
					INTERNAL CAPTURE %
					15%

**PROPOSED REZONING PLAN**

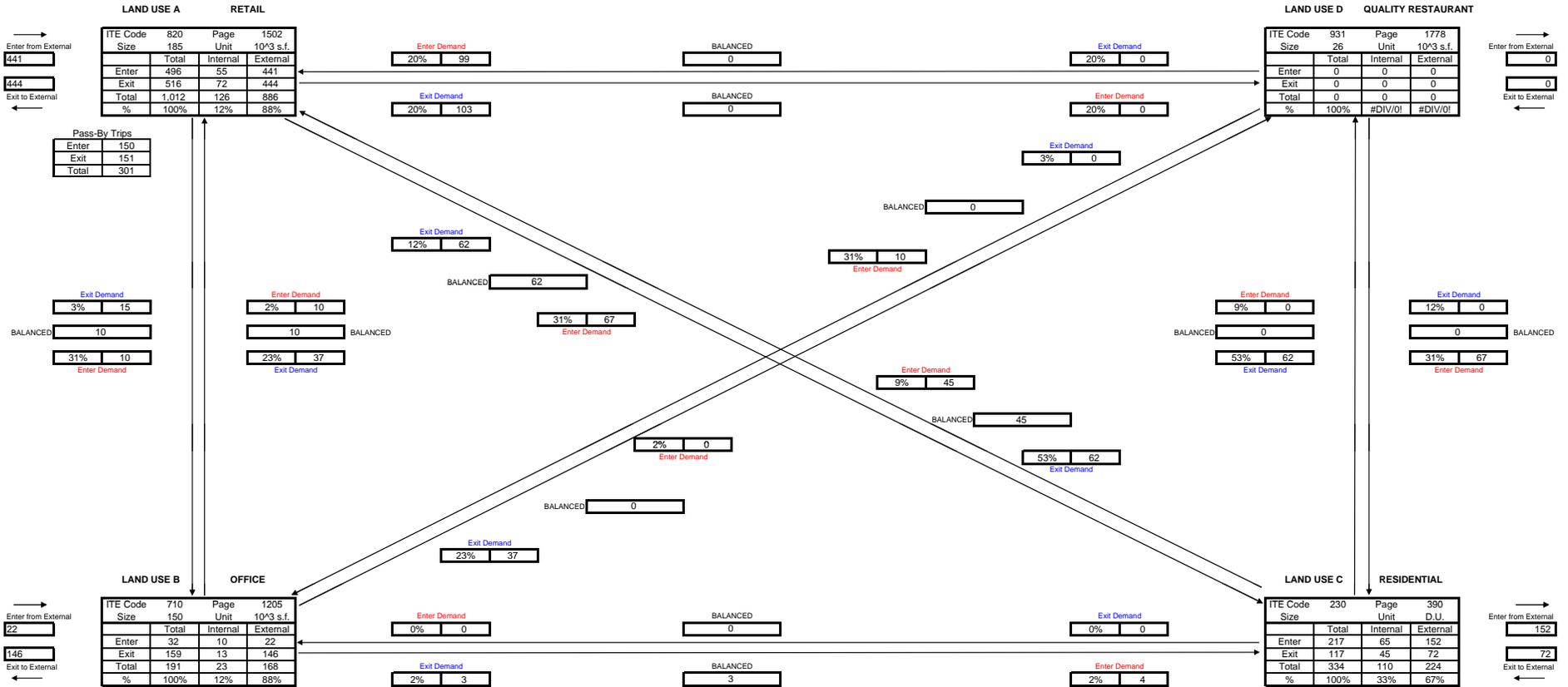
**DAILY TRIPS - INTERNAL CAPTURE**



NET EXTERNAL TRIPS FOR MULTI-USE DEVELOPMENT					
	RETAIL	OFFICE	RESIDENTIAL	MISCELLANEOUS	TOTAL
Enter	4,702	567	1,205	0	6,473
Exit	4,642	507	1,324	0	6,473
Total	9,344	1,074	2,529	0	12,947
Single-Use Trip Gen. Estm.	10,656	1,334	3,608	0	15,598
					INTERNAL CAPTURE %
					17%

**PROPOSED REZONING PLAN**

**PM Peak Hour of Adjacent Street**



NET EXTERNAL TRIPS FOR MULTI-USE DEVELOPMENT					
	RETAIL	OFFICE	RESIDENTIAL	MISCELLANEOUS	TOTAL
Enter	441	22	152	0	615
Exit	444	146	72	0	662
Total	886	168	224	0	1,278

INTERNAL CAPTURE %

2-9-11

**PROPOSED REZONING OF PIN NOS.  
0796302996, 0796308642, and 0796314150  
(THE "PROPOSED ZONING CASE")**

**Approximately 11.74 Acres – Crabtree Valley Avenue at Creedmoor Road**

**REPORT OF MARCH 7, 2011 NEIGHBORHOOD MEETING**

In accordance with Section 10-2165(b)(3) of the Raleigh City Code, a neighborhood meeting was held with respect to the Proposed Zoning Case at 6:00 p.m. on Monday, March 7, 2011 at the Raleigh Police Northwest District Station at 8016 Glenwood Avenue in Raleigh. Attached as Exhibit A is a list of those persons and organizations contacted about the meeting. Those persons and organizations were mailed a letter of invitation concerning the meeting, a copy of which is attached as Exhibit B. The letters were mailed on or about February 21, 2011 via First Class U.S. Mail.

Attached as Exhibit C is a list of the persons in attendance at the meeting. The issues discussed at the meeting included identification of the specific property subject to the rezoning request, the current zoning of the property, and modifications of such zoning to be requested in the Proposed Zoning Case. Also discussed were the nature of improvements planned for the property proposed for rezoning and the effect upon such property, as well as upon neighboring properties, of the proposed realignment of Crabtree Valley Avenue. There have been no changes to the rezoning petition subsequent to the neighborhood meeting.

A copy of this report will be provided to the Planning Department upon the filing of the petition for the Proposed Zoning Case.

Respectfully submitted, this 18<sup>th</sup> day of March, 2011.

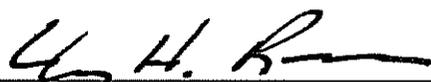
  
\_\_\_\_\_  
Lacy H. Reaves  
Attorney for the Petitioner

EXHIBIT A

PIN_NUM	OWNER	ADDR1	ADDR2	ADDR3
0796301789	CNL RETIREMENT SUN1 LP HCP SUNY ✓	DELOITTE TAX LLP ATTN: HCP1	2235 FARADAY AVE STE O	CARLSBAD CA 92008-7215
0795399646	PINNACLE APARTMENTS LLC ✓	DRUCKER & FALK	11824 FISHING POINT DR	NEWPORT NEWS VA 23606-2679
0796309117	WILLIAMS, JAMES ✓	5004 EDWARDS MILL RD	RALEIGH NC 27612-4420	
0796308224	LEWIS, MICHAEL W & JEANNETTE S ✓	8117 PONY PASTURE CT	RALEIGH NC 27612-7379	
0795497805	CVM ASSOCIATES ✓	PO BOX 31147	RALEIGH NC 27622-1147	
0796312398	LANGLEY, VICTORIA M MARTIN, SIDNEY A ✓	717 BLENHHEIM DR	RALEIGH NC 27612-4902	
0796502569	CVM HOLDINGS LLC ✓	PO BOX 31147	RALEIGH NC 27622-1147	
0796303474	GRANTHAM, CHARLES E ✓	3909 RHODODENDRON DR	RALEIGH NC 27612-5430	
0796218350	NATIONWIDE MUTUAL INSURANCE COMPANY ✓	ONE NATIONWIDE PLAZA	MAIL CODE 1-03-304	COLUMBUS OH 43215
0796305476	GRANTHAM, CHARLES EDWARD ✓	3909 RHODODENDRON DR	RALEIGH NC 27612-5430	
0796404276	CVM HOLDINGS LLC ✓	PO BOX 31147	RALEIGH NC 27622-1147	
0796304477	GRANTHAM, CHARLES E ✓	3909 RHODODENDRON DR	RALEIGH NC 27612-5430	
0796307332	GRANTHAM, CHARLES EDWARD ✓	3909 RHODODENDRON DR	RALEIGH NC 27612-5430	
0796408142	N.C DEPARTMENT OF TRANSPORTATION ✓	PO BOX 15580	DURHAM NC 27704-0580	
0796306359	REESE, WILLIAM E II & JENNIFER S ✓	5300 LAKE EDGE DR	HOLLY SPRINGS NC 27540-9339	



**LEGEND**

-  Parcel 1
-  Parcel 2



 <b>RAMEY KEMP ASSOCIATES</b>	
<i>Crabtree Place Development Raleigh, North Carolina</i>	
Site Location Map	
	Figure 1

**Trip Generation Comparison (Existing Zoning vs. Proposed Zoning)**

Existing Zoning	Land Uses	Size	Daily Trips		Weekday AM Peak Hour		Weekday PM Peak Hour	
			Enter	Exit	Enter	Exit	Enter	Exit
CH Engineering (Parcel 2)	(see attached sheet from CH)		7517	7517	-	-	527	521
Parcel 1	820 - Shopping Center	10,920 SF	805	805	25	16	71	74
Pass-By Trips (Parcel 1)							-24	-25
<b>Totals</b>			<b>8322</b>	<b>8322</b>	<b>N/A</b>	<b>N/A</b>	<b>574</b>	<b>570</b>
			<b>16,644</b>		<b>N/A</b>		<b>1,144</b>	

\*The CH Engineering traffic volumes are based on Scenario 4 and assume an internal capture rate of 17% (overall) and a pass-by rate of 14% (overall).

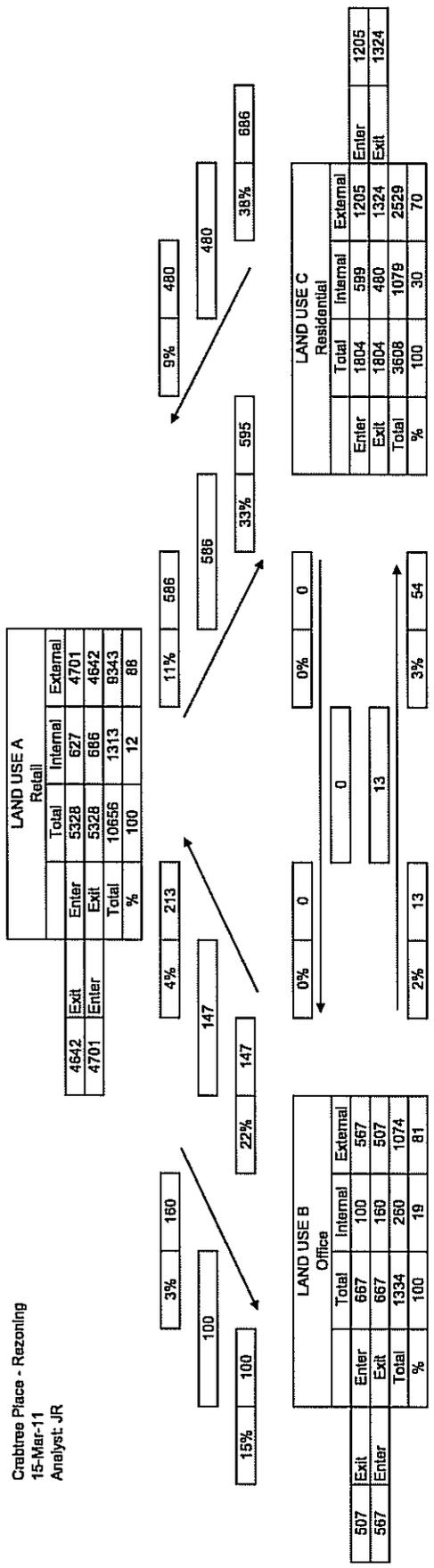
\*The Steak & Ale Parcel is 0.91 acres and assumed a development ratio 12,000 sq. ft./acre.

Proposed Zoning	Land Uses	Size	Daily Trips		Weekday AM Peak Hour		Weekday PM Peak Hour	
			Enter	Exit	Enter	Exit	Enter	Exit
Trips Generated	220 - Apartments	575 Units	1804	1804	57	228	217	117
	710 - General Office Building	100,000 SF	667	667	165	23	32	158
	820 - Shopping Center	200,000 SF	5328	5328	141	90	496	516
	312 - Business Hotel	200 Rooms	727	727	68	48	74	50
Internal Capture (Apartments)			-599	-480			-65	-45
Internal Capture (General Office Building)			-100	-160			-10	-13
Internal Capture (Shopping Center)			-627	-686			-55	-72
Pass-By Trips (Shopping Center)							-150	-151
<b>Totals</b>			<b>7200</b>	<b>7200</b>	<b>431</b>	<b>389</b>	<b>539</b>	<b>560</b>
			<b>14,400</b>		<b>820</b>		<b>1,099</b>	

11-9-2

DAILY INTERNAL CAPTURE CALCULATION - PROPOSED REZONING

Crabtree Place - Rezoning  
 15-Mar-11  
 Analyst: JR



Net External Trips for Multi-Use Development

	LAND USE A	LAND USE B	LAND USE C	TOTAL
Enter	4701	567	1205	6473
Exit	4642	507	1324	6473
Total	9343	1074	2529	12946
Single-Use Trip Gen. Est.	10656	1334	3608	15598
			INTERNAL CAPTURE	17%

PM PEAK HOUR INTERNAL CAPTURE CALCULATION - PROPOSED REZONING

Crabtree Place - Rezoning  
 15-Mar-11  
 Analyst: JR

LAND USE A Retail			
	Total	Internal	External
Enter	496	55	441
Exit	516	72	444
Total	1012	127	885
%	100	13	87

444	Exit
441	Enter



LAND USE B Office			
	Total	Internal	External
Enter	32	10	22
Exit	158	13	145
Total	190	23	167
%	100	12	88

145	Exit
22	Enter

0	0	0
0	0	0
3	3	3
2%	2%	2%

LAND USE C Residential			
	Total	Internal	External
Enter	217	65	152
Exit	117	45	72
Total	334	110	224
%	100	33	67

152	Enter
72	Exit

Net External Trips for Multi-Use Development				
	LAND USE A	LAND USE B	LAND USE C	TOTAL
Enter	441	22	152	615
Exit	444	145	72	661
Total	885	167	224	1276
Single-Use Trip Gen. Est.	1012	190	334	1536
			INTERNAL CAPTURE	17%